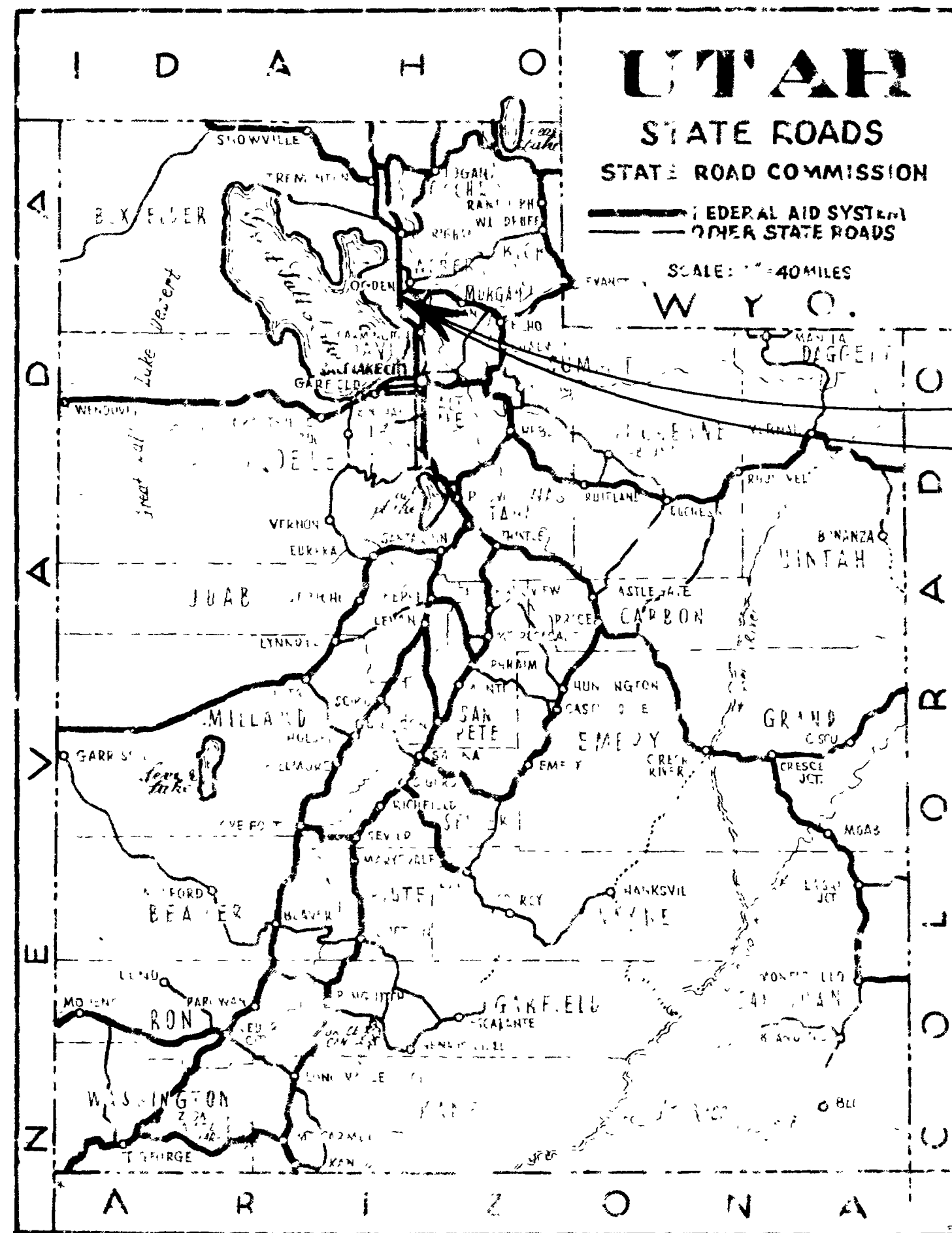


# STATE OF UTAH STATE ROAD COMMISSION



1-15-7(58) 332  
1-15-8(20) 334  
1G-15-8(18) 334

## PLANS OF PROPOSED STATE ROAD

FEDERAL AID PROJECT

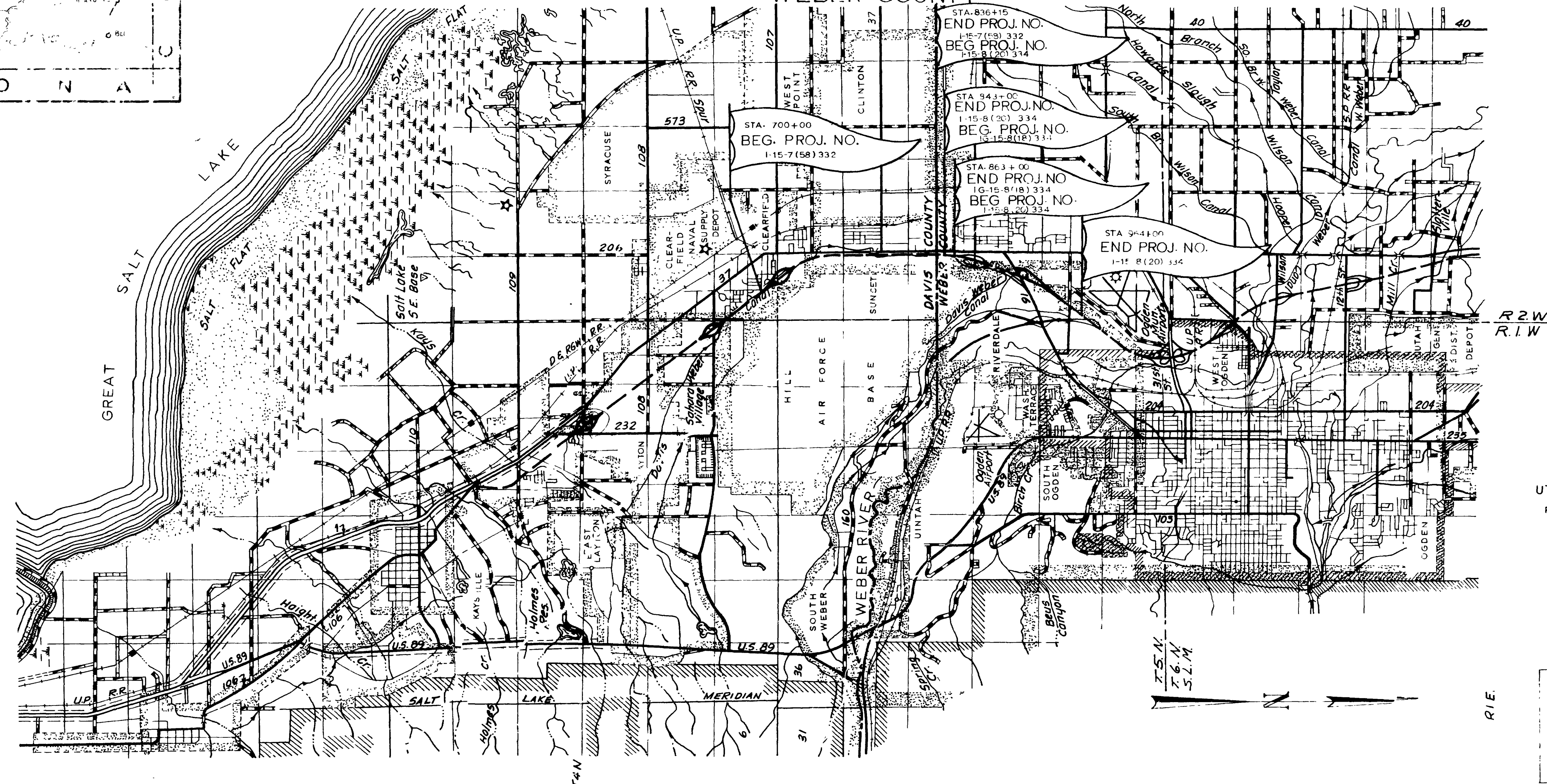
1-15-7 (58) 332  
LENGTH = 2.585 MILES  
DAVIS COUNTY

1-15-8 (20) 334  
LENGTH = 2.043 MILES  
WEBER COUNTY

1G-15-8(18) 334  
LENGTH = 0.380 MILES  
WEBER COUNTY

### INDEX TO SHEETS

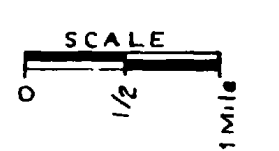
| NO.     | DESCRIPTION                   | NO.     | NO.       |
|---------|-------------------------------|---------|-----------|
| 1       | TITLE SHEET                   |         |           |
| 2, A-C  | TYPICAL SECTIONS & MATLS. SH. |         |           |
| 3, A-E  | SUMMARY SHEETS                |         |           |
| 4-31    | PLAN AND PROFILE              |         |           |
| 32-44   | HILL FIELD OVERPASS           | D-719   | 1201401   |
| 45-69   | UPRR. OVERPASS                | F-80    | 854170    |
| 70-95   | 5600 SOUTH OVERPASS           | F-66    | 8594730   |
| 96-101  | 5600 SOUTH CANAL X-ING        | D-744   | 121075200 |
| 102-131 | RIVERDALE ROAD UNDERPASS      | C-453   | 90114300  |
| 132-145 | 4400 SOUTH UNDERPASS          | D-735   | 95512804  |
| 146     | STD. RAILING DETAILS          | MR-1    |           |
| 147     | CONCRETE PILES                | SP-1    |           |
| 148     | STD. STEEL DETAILS            | SS-3    |           |
| 149     | STD. CATCH BASINS             | V-752   |           |
| 150     | STD. CATCH BASINS             | V-753   |           |
| 151     | STD. CLEANOUT BOXES           | V-777   |           |
| 152-156 | STD. ROAD SIGNS               | 500-1-5 |           |
| 157     | STD. FLARED END SEC. RCP.     | B-160-1 |           |
| 158     | RIGHT OF WAY MARKERS          | B-164   |           |
| 159     | SUPERELEVATION & WIDENING     | 202     |           |
| 160     | LEFT TURN MEDIANS             | 211     |           |
| 161     | CONCRETE CURB & GUTTER        | 220     |           |
| 162     | DEEP BEAM GUARD RAIL          | 230     |           |
| 163     | RCP. CULVERTS                 | 320     |           |
| 164     | DROP INLETS                   | 346     |           |
| 165     | CONSTRUCTION IDENT. SIGNS     | 504     |           |
| 166     | CHAIN LINK FENCE              | 523     |           |



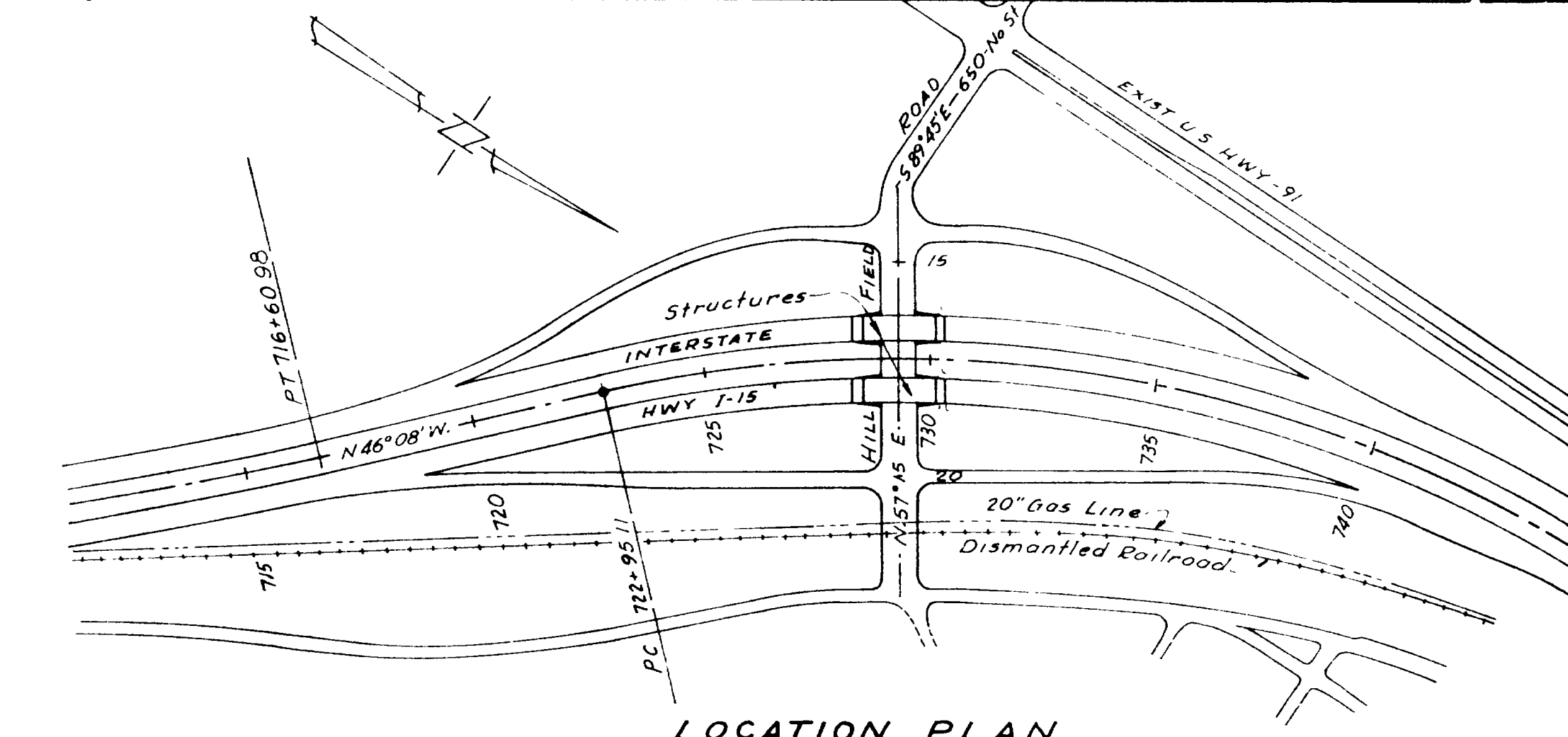
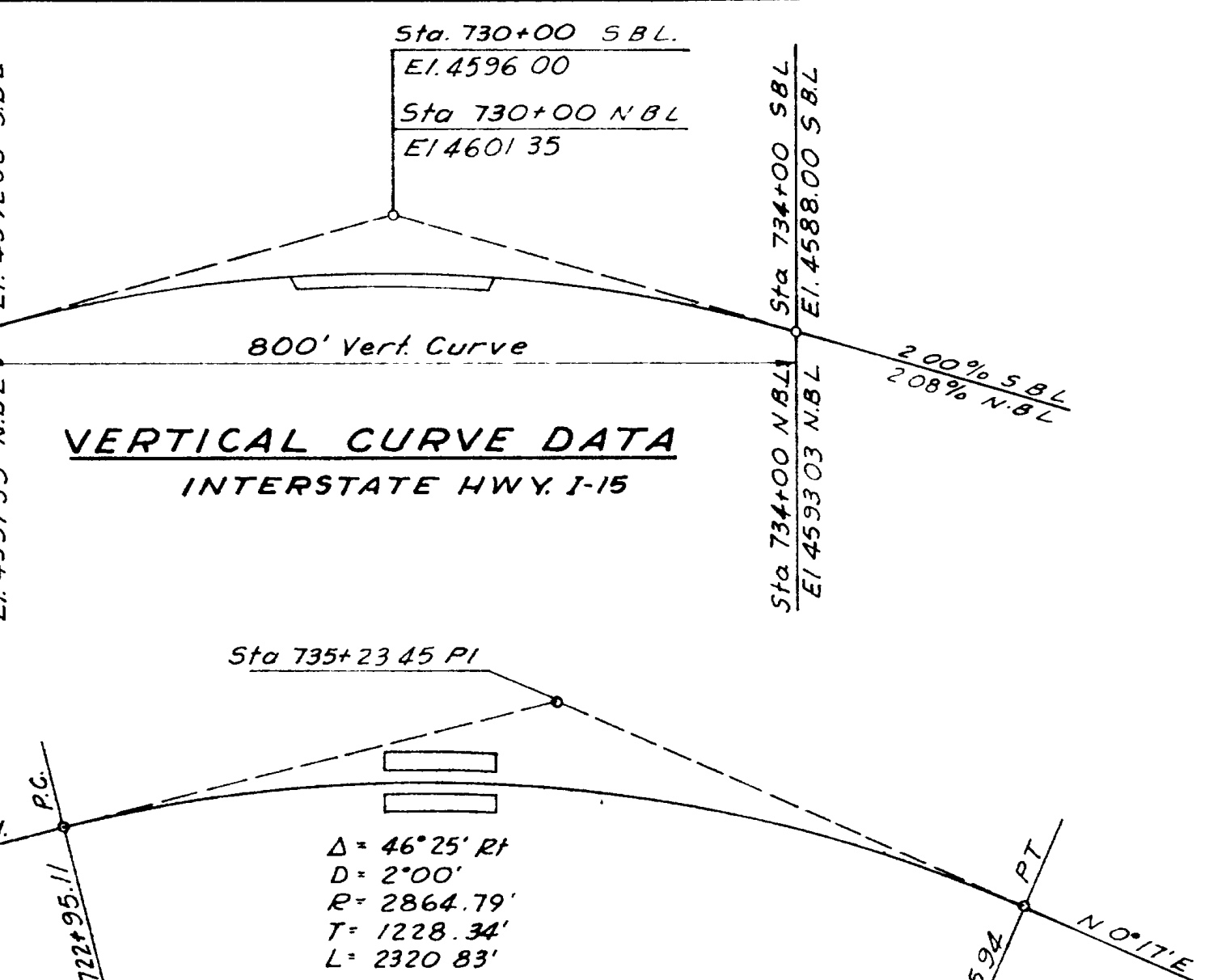
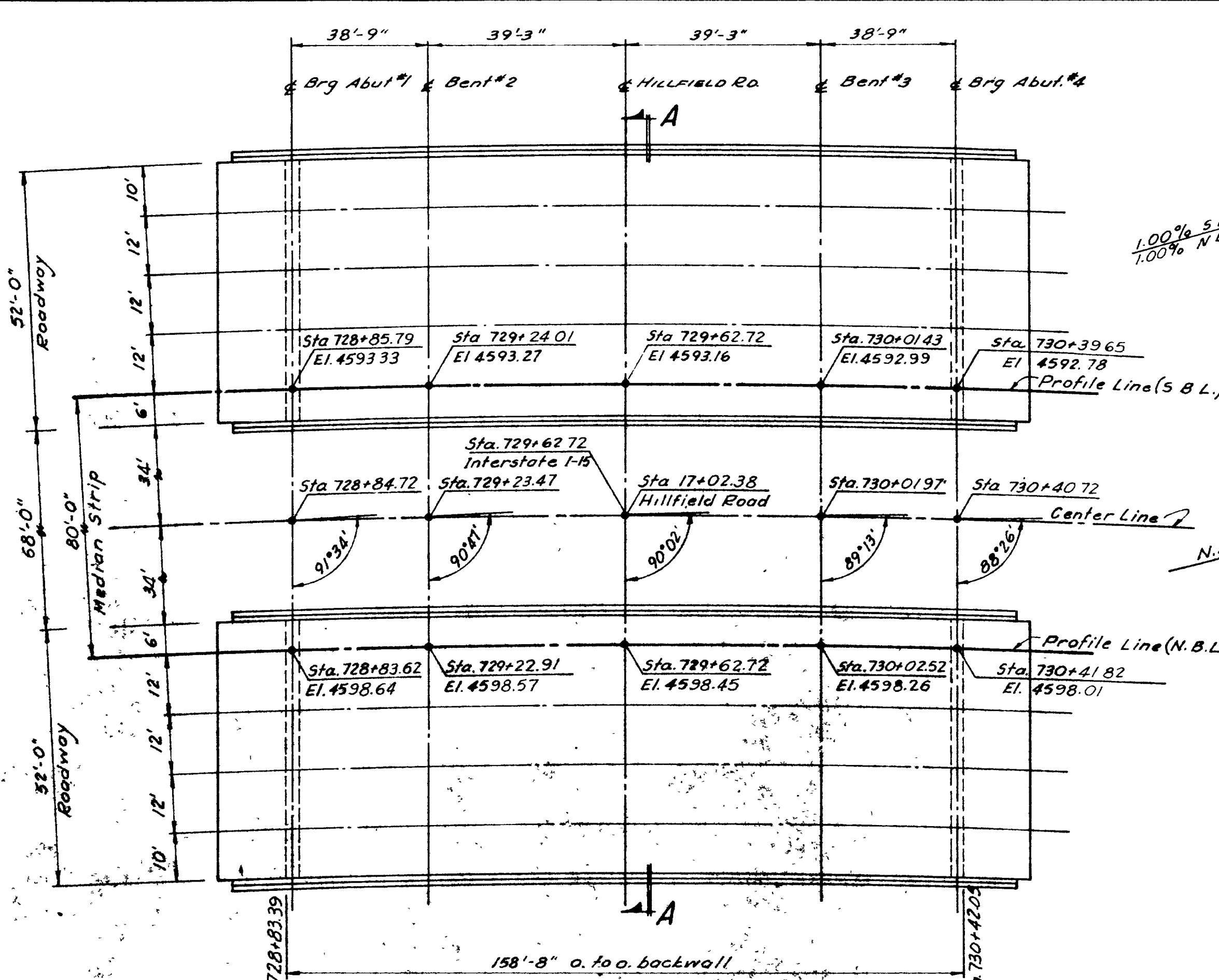
R 2 W  
R 1 W

UTAH STATE DEPARTMENT OF HIGHWAY  
RECOMMENDED FOR APPROVAL: AUG. 1963  
*Blaine J. Kay*  
ENGINEER OF ROADWAY DESIGN  
RECOMMENDED FOR APPROVAL: AUG. 1963  
*Blaine J. Kay*  
ENGINEER OF PLANS AND CONTRACTS  
APPROVED: AUG. 1963  
*Blaine J. Kay*  
ACT. DIRECTOR OF HIGHWAYS

DEPARTMENT OF COMMERCE  
BUREAU OF PUBLIC ROADS  
APPROVED: \_\_\_\_\_  
DIVISION ENGINEER DATE

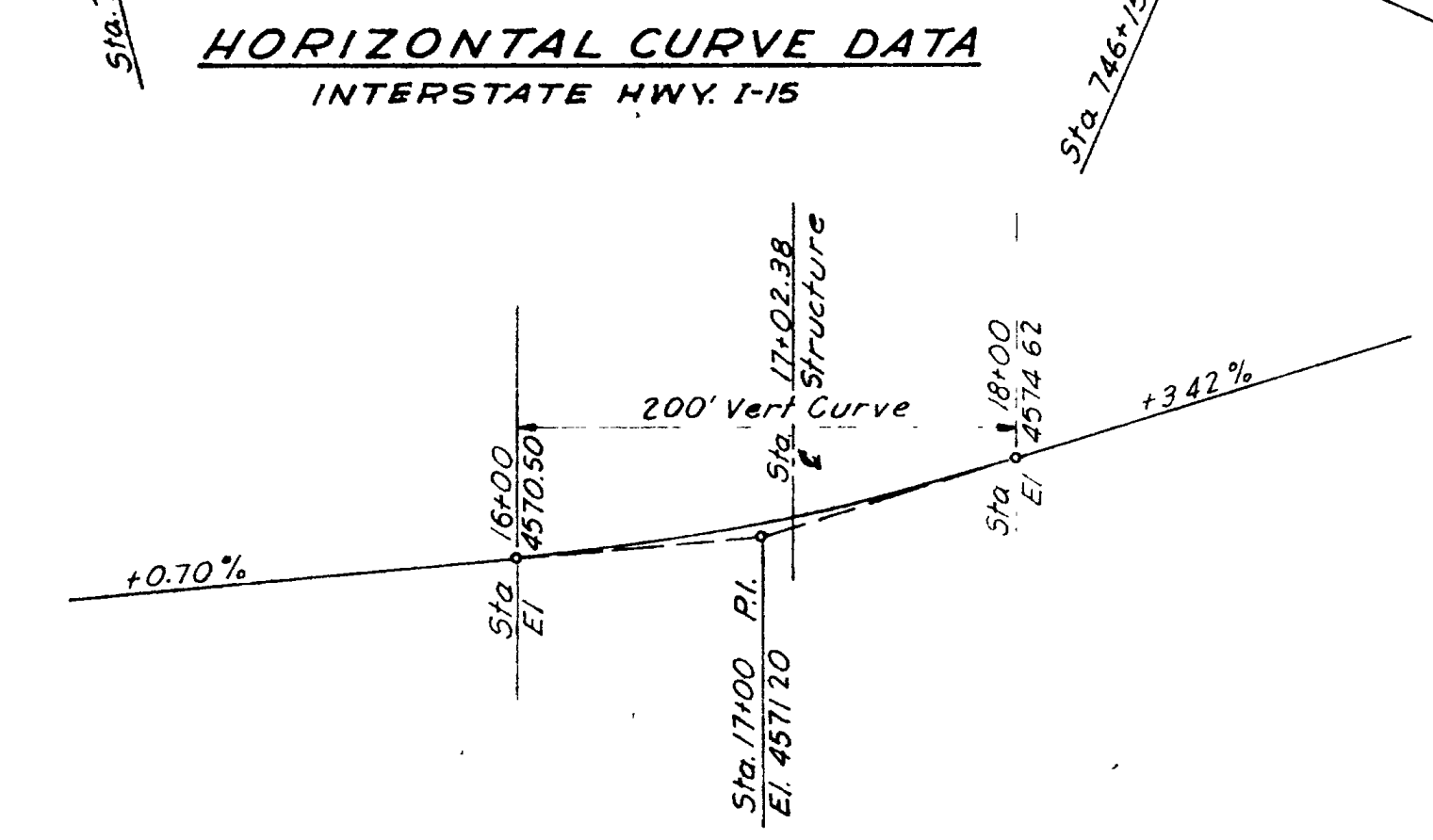






**GENERAL NOTES**

- 1- Materials, construction and workmanship shall be in accordance with Utah State Standard Specifications for Road and Bridge Construction, 1960 Edition and supplements thereto which are in effect at the date of request for bids.
- 2- All reinforcing steel shall be intermediate grade billet steel, conforming to A.A.S.H.O. designation M 31. Deformations shall conform to A.A.S.H.O. designation M 37.
- 3- All structural steel shall be structural carbon steel conforming to A.A.S.H.O. designation M 183 (ASTM A36).
- 4- Exposed corners at joint filler and open joints shall be beveled  $\frac{3}{4}$ " or rounded.



**DESIGN DATA**

H20-516-44 or Interstate Alternate Loading in accordance with A.A.S.H.O. Specifications of 1961, and Interim Specifications

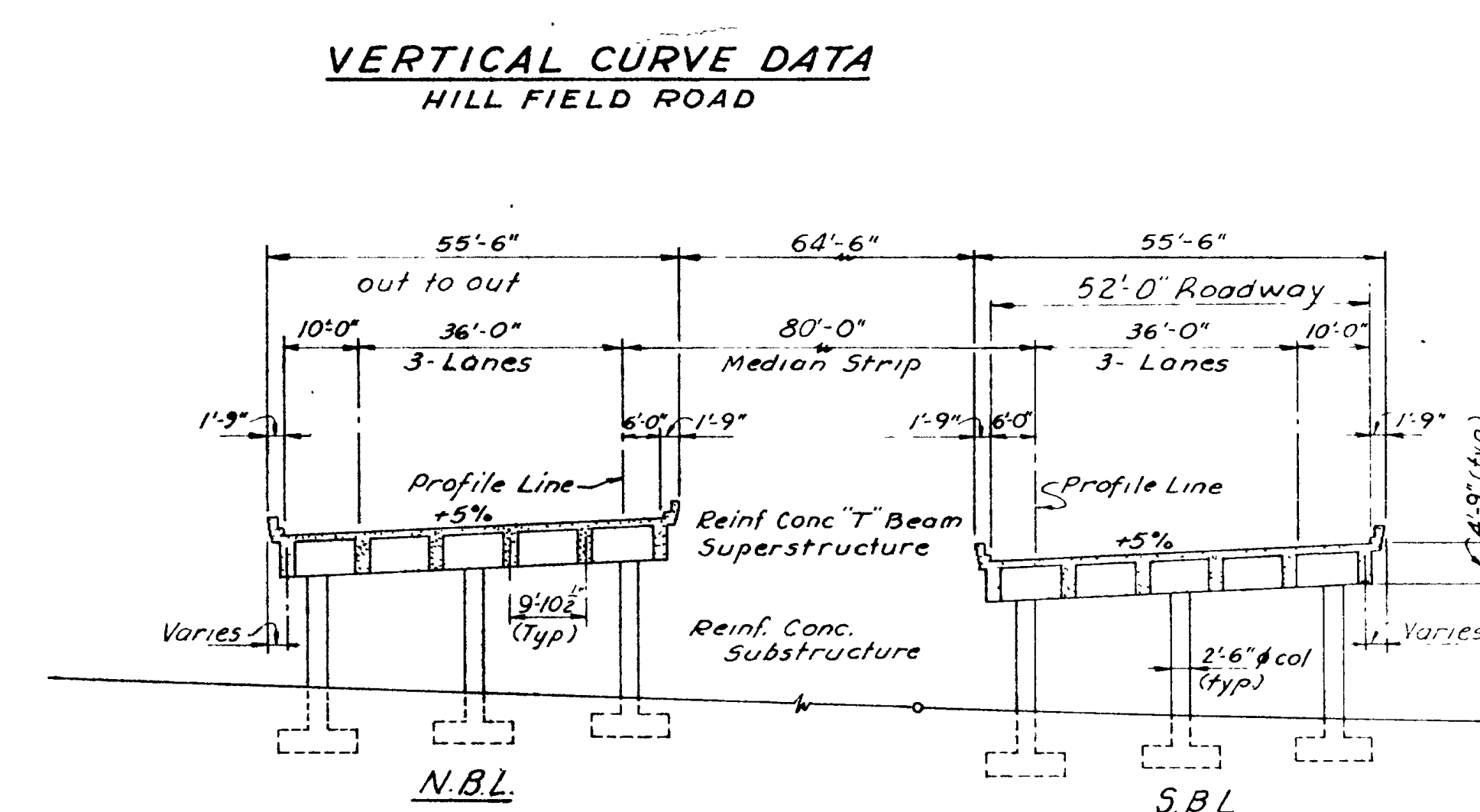
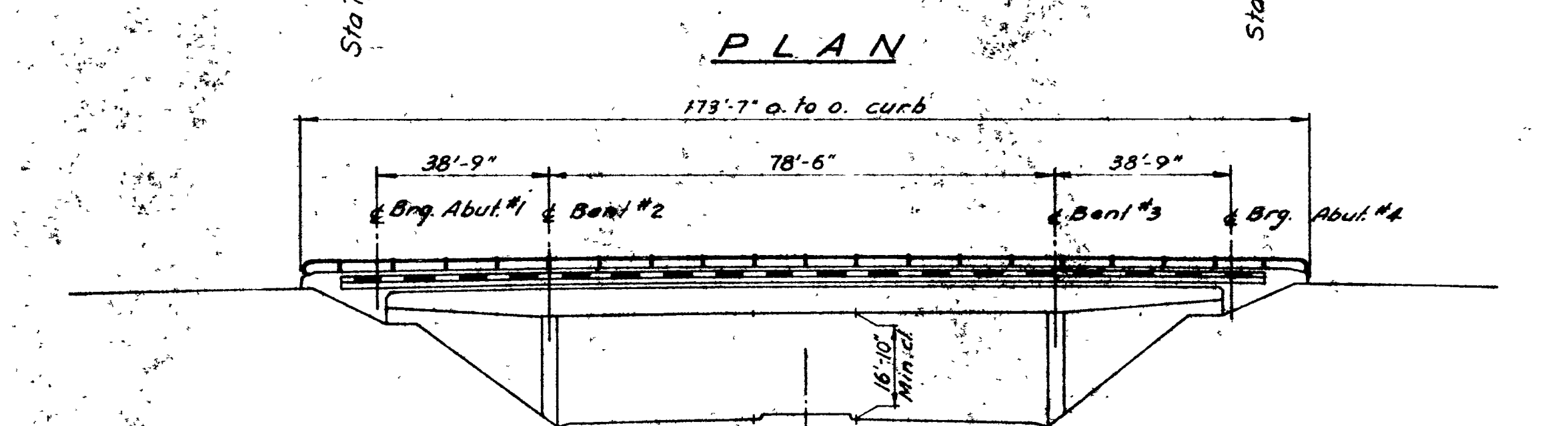
Cast in place Concrete.  $f_c = 1200$  p.s.i.

$f_s$  (Reinf.) = 20,000 p.s.i.

$n = 10$

Wearing Surface  $\frac{3}{4}$ " Concrete

15#/sq ft Future wearing surface



**ITEM**

Excavation for Structures (Unclass)

Class A Concrete (AE)

Structural Steel

Reinforcing Steel

Metal Railing (single rail)

Pile (other than timber)

Furnish Pile Driving Equipment

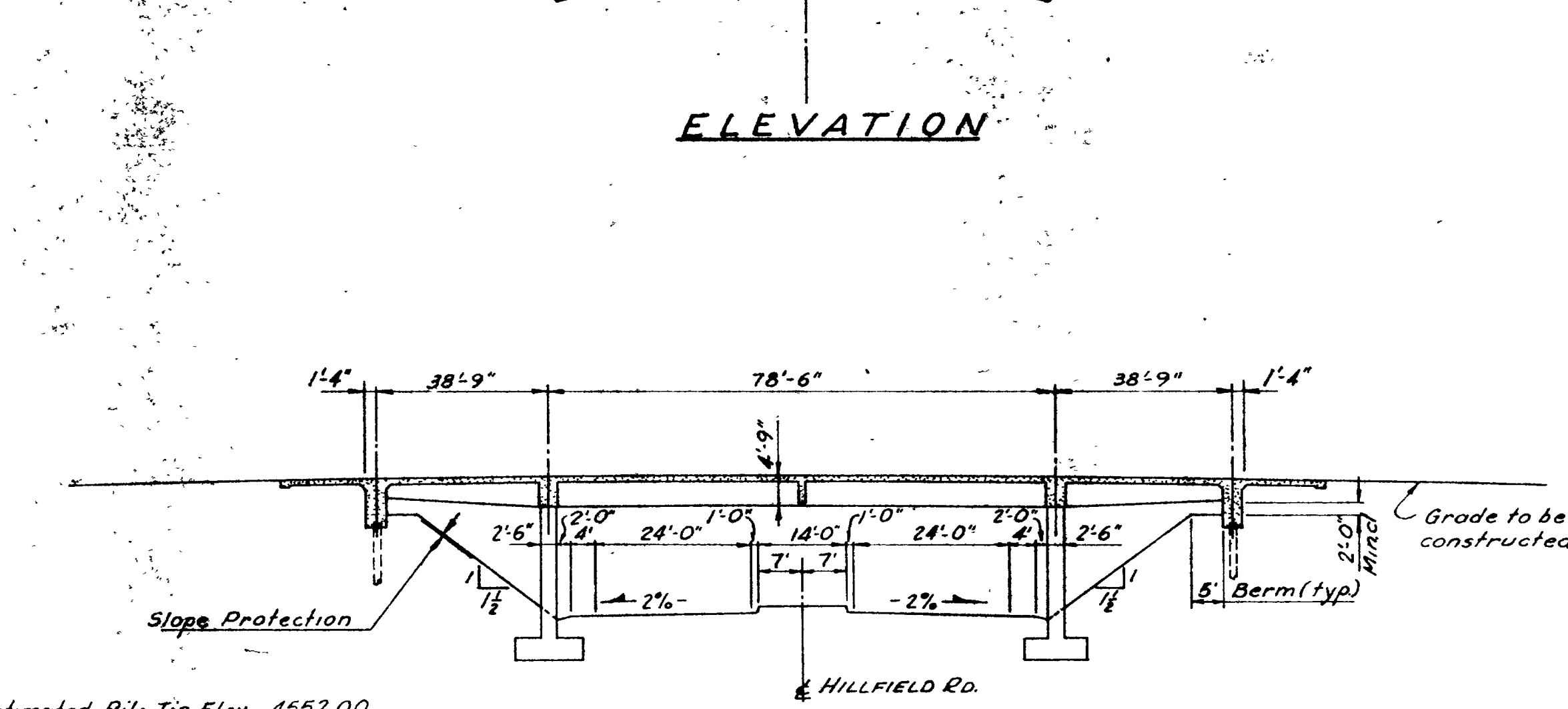
Mechanical Tamping

Reinf. Concrete Slope Protection

Electrical Work Bridges

**QUANTITIES**

| ITEM                                | ESTIMATED | AS CONSTRUCT. | UNIT   |
|-------------------------------------|-----------|---------------|--------|
| Excavation for Structures (Unclass) | 52.4      | 418.1         | cu yd  |
| Class A Concrete (AE)               | 1,455     | 1437.5        | cu yd  |
| Structural Steel                    | 2,709     | 2709.0        | lb     |
| Reinforcing Steel                   | 351,507   | 331,507.0     | lb     |
| Metal Railing (single rail)         | 689       | 689.0         | lin ft |
| Pile (other than timber)            | 765       | 774.4         | lin ft |
| Furnish Pile Driving Equipment      | 1         | 100%          | lump   |
| Mechanical Tamping                  | 24        | 28.0          | hour   |
| Reinf. Concrete Slope Protection    | 1,337     | 1471.2        | sq yd  |
| Electrical Work Bridges             | 1         | 100%          | lump   |



- INDEX OF SHEETS**
- 1- LAYOUT & SITUATION
  - 2- FOUNDATION PLAN
  - 3- SOIL DATA
  - 4- DECK PLAN & SECTIONS
  - 5- ABUTMENT NO 1 & 4 DETAILS
  - 6- BENT DETAILS
  - 7- BEAM DETAILS
  - 8- BEAM FRAMING PLAN
  - 9- SCREED ELEVATIONS
  - 10- PARAPET & RAIL DETAILS
  - 11- REINF. CONC. SLOPE PROTECTION
  - 12- REINF. STEEL SCHEDULE
  - 13- LIGHTING DETAILS FOR STRUCTURE
  - SP-1 STANDARD PILE DETAILS
  - MR-1 STANDARD RAILING DETAILS

UTAH STATE DEPARTMENT OF HIGHWAYS

STRUCTURES DIVISION

HILLFIELD INTERCHANGE TO DAVIS-WEBER CO. LINE

HILLFIELD RD INTERCH. O'PASS

LAYOUT & SITUATION

| NO. | DATE | BY  | REVISIONS |
|-----|------|-----|-----------|
| 1   |      | AW  |           |
| 2   |      | JHO |           |
| 3   |      | HJS |           |
| 4   |      | BMW |           |

729+62.72

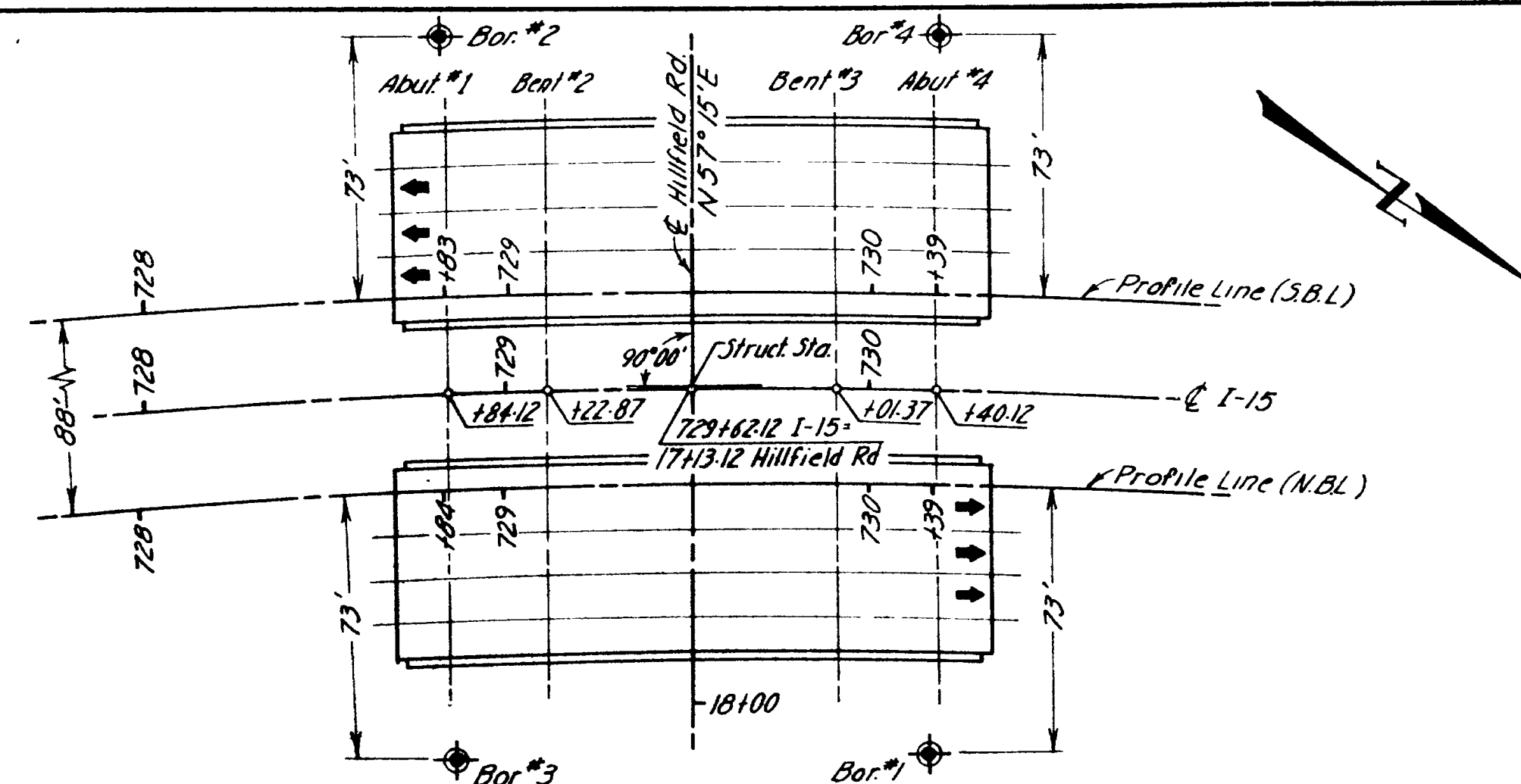
DAVIS

D-719 1 of 13

Estimated Pile Top Elev. 4552.00

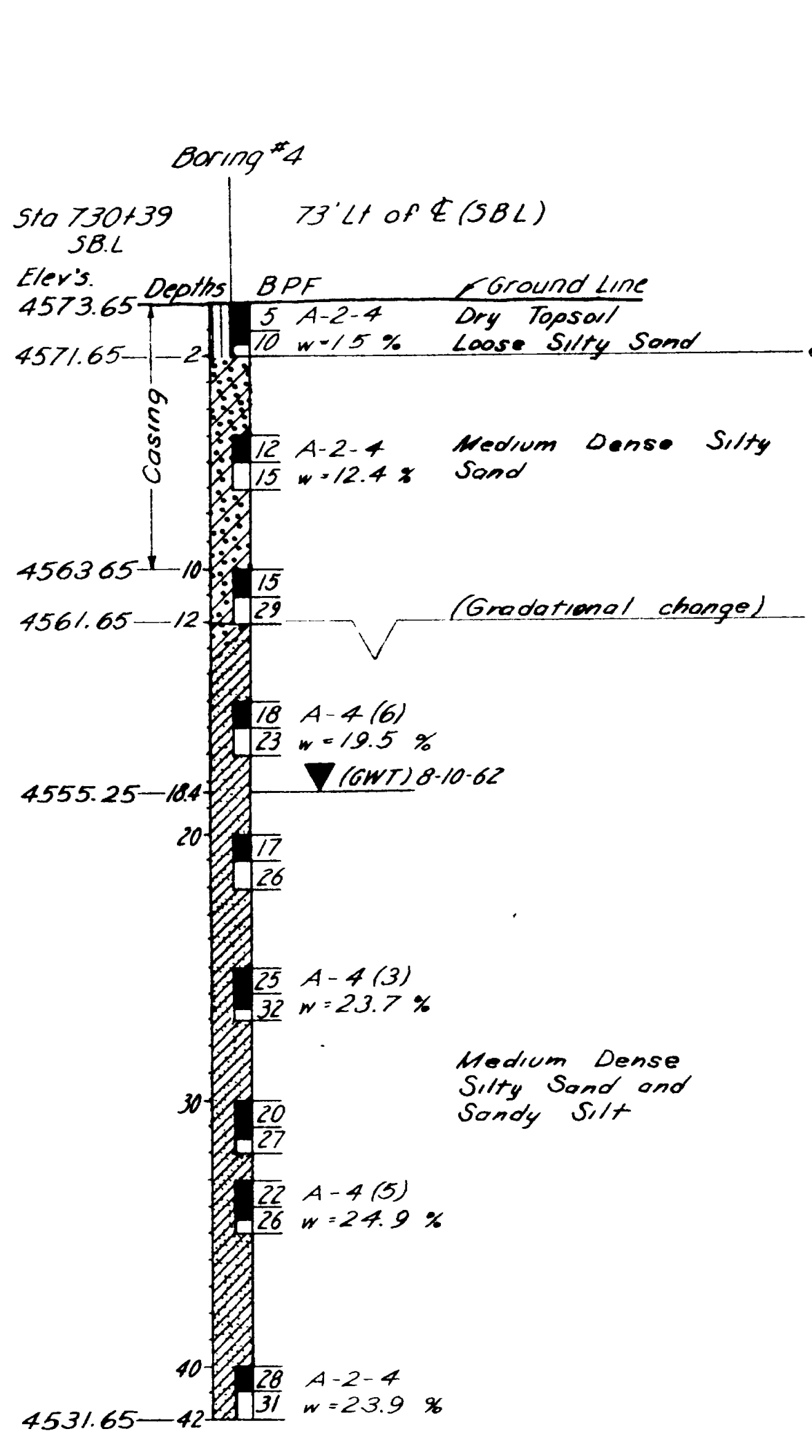
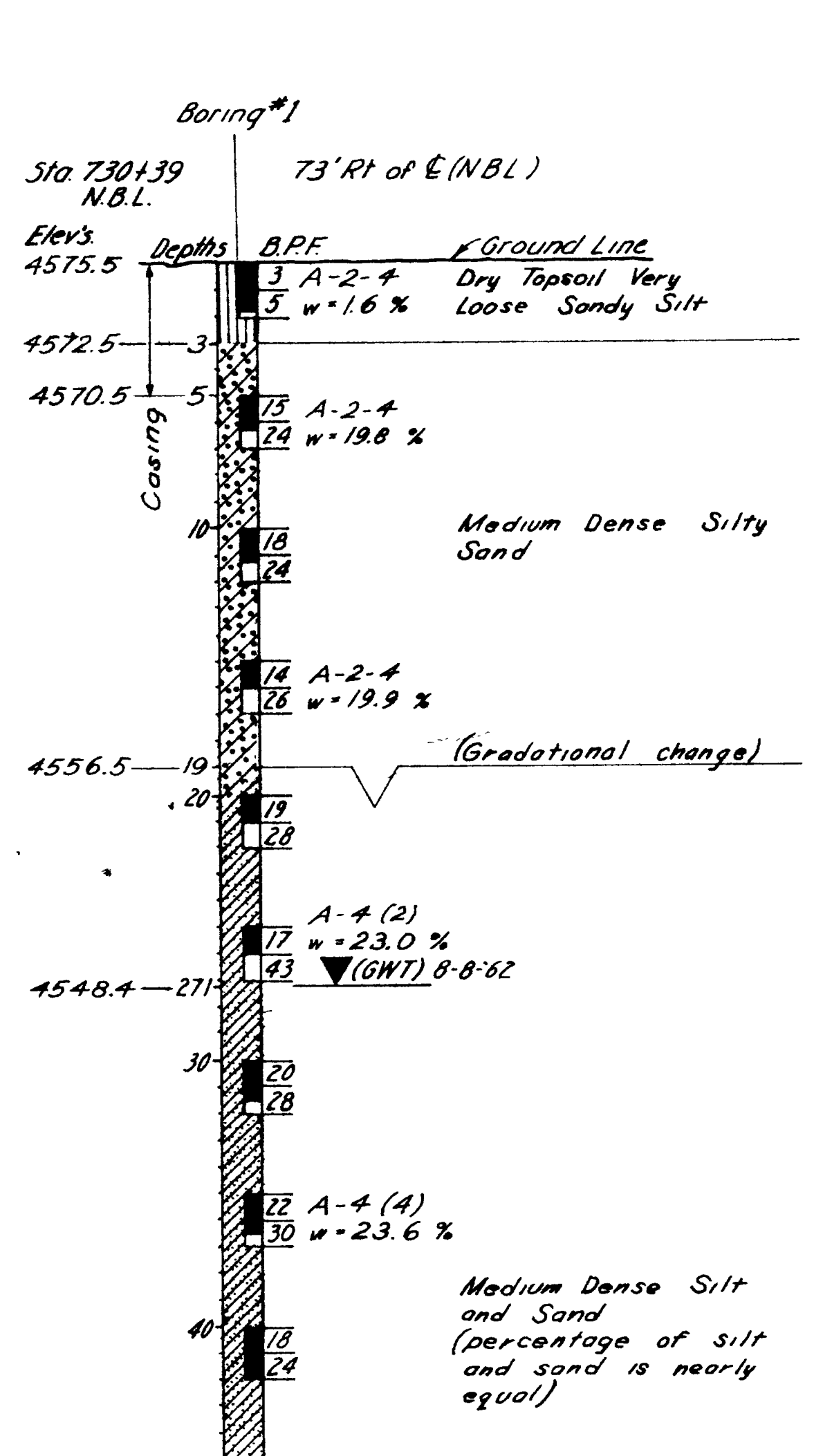
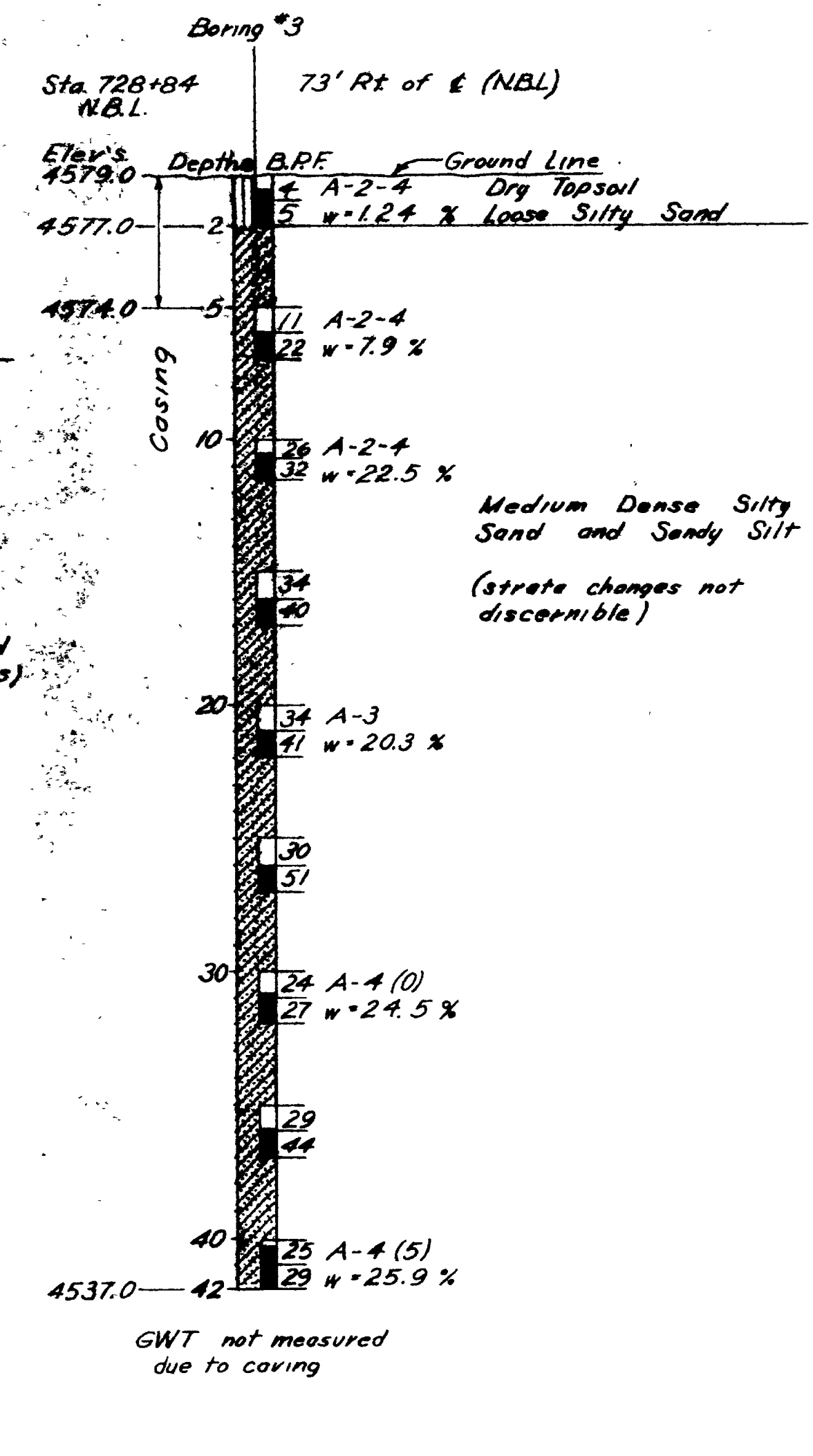
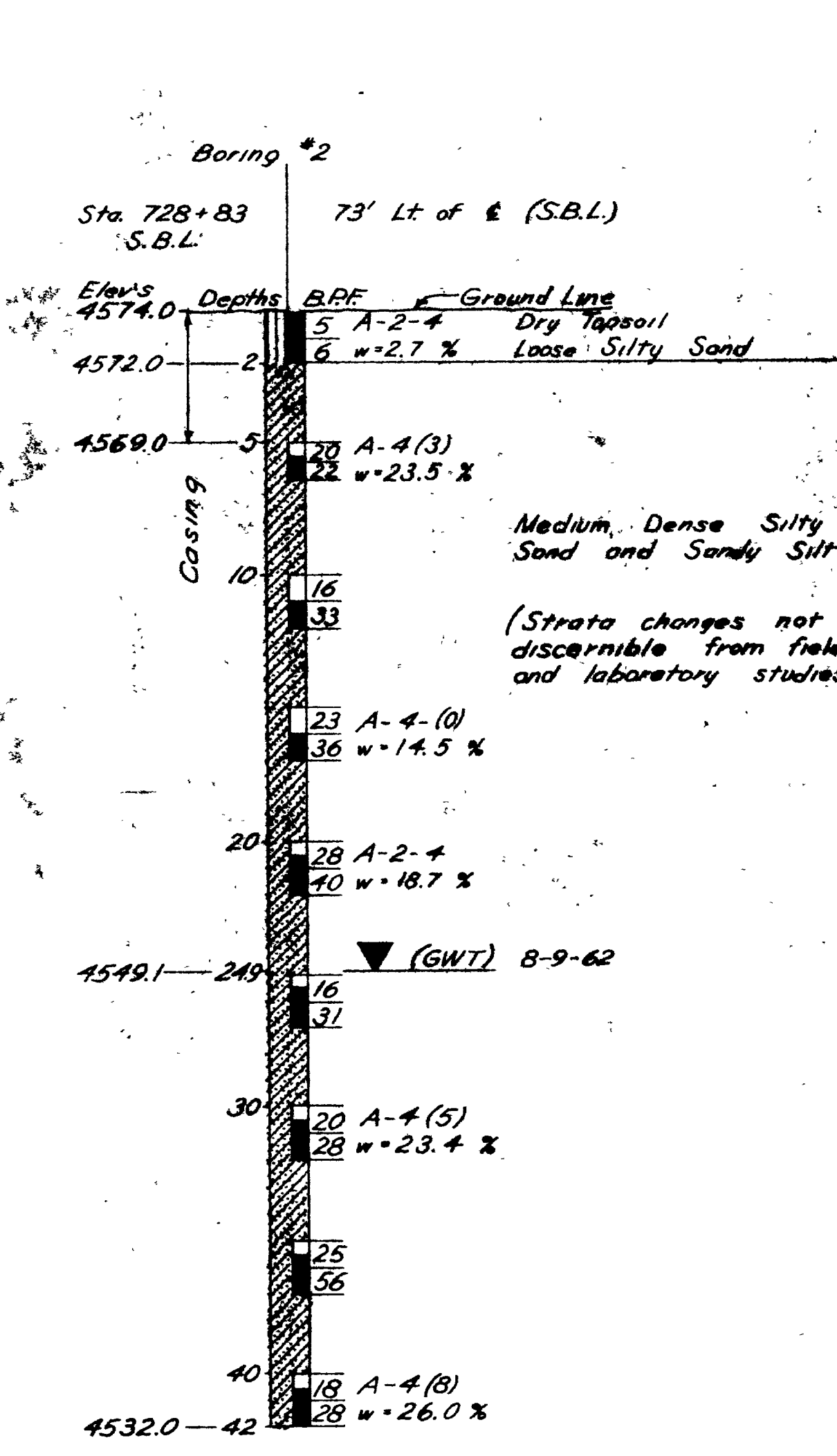






Plan  
Scale: 1" = 40'

- KEY TO BORING LOGS**
- RELATIVE DENSITY (SAND & SILT)**
- VERY LOOSE - LESS THAN 4 BLOWS PER FOOT
  - LOOSE - 4 TO 10 BLOWS PER FOOT
  - MEDIUM - 10 TO 30 BLOWS PER FOOT
  - DENSE - 30 TO 50 BLOWS PER FOOT
  - VERY DENSE - MORE THAN 50 BLOWS PER FOOT
- CONSISTENCY (CLAY)**
- VERY SOFT - LESS THAN 2 BLOWS PER FOOT
  - SOFT - 2 TO 4 BLOWS PER FOOT
  - MEDIUM - 4 TO 8 BLOWS PER FOOT
  - STIFF - 8 TO 15 BLOWS PER FOOT
  - VERY STIFF - 15 TO 30 BLOWS PER FOOT
  - HARD - MORE THAN 30 BLOWS PER FOOT
- |                           |                       |                      |
|---------------------------|-----------------------|----------------------|
| [Symbol] TOP SOIL OR FILL | [Symbol] IGNEOUS      | [Symbol] SANDY CLAY  |
| [Symbol] GRAVEL           | [Symbol] LIMESTONE    | [Symbol] CLAYEY SAND |
| [Symbol] SAND             | [Symbol] CONGLOMERATE | [Symbol] SILTY CLAY  |
| [Symbol] SILT             | [Symbol] DOLOMITE     | [Symbol] CLAYEY SILT |
| [Symbol] CLAY             |                       | [Symbol] SILTY SAND  |
| [Symbol] SHALE            |                       | [Symbol] SAND SILT   |



Profile  
Scale: 1" = 5'

**BORING (TEST HOLES) NO.**

**STATION** 0+00 E OR LT OR RT IN FT. OFFSET.

| ELEVATIONS           | DEPTHS | GR. EL. 4562 FT. |
|----------------------|--------|------------------|
| GROUND ELEVATION     |        |                  |
| 4555                 | 5      |                  |
| GROUND WATER TABLE   |        | 4552             |
| 4552                 | 7      |                  |
| STRATA CHANGE        |        | 4546             |
| 4546                 | 10     |                  |
| LOCATION OF SAMPLE   |        | 20               |
| 20                   | 14     |                  |
| 25                   | 18     |                  |
| SAMPLE NOT RECOVERED |        | 25               |
| 30                   | 29     |                  |
| BOTTOM OF BORING     |        | 4531             |
| 4531                 | 30     |                  |

EXAMPLE TYPICAL STIFF MEDIUM PLASTIC BRN CLAY, SOME SILT A-6(8) W=9%

DATE

THIN WALL SHELBY TUBE UNDISTURBED SAMPLER USED.

SPLIT BARREL UNDISTURBED SAMPLER WITH LINER RINGS OR CALIFORNIA TYPE SAMPLER

REASON NOT RECOVERED

CLASSIFICATION OF EACH SAMPLE AND RESULTS OF CLASSIFICATION TESTS.

- ABBREVIATIONS**
- L.L. - LIQUID LIMIT %
  - P.I. - PLASTIC INDEX %
  - w - NATURAL MOISTURE CONTENT IN %
  - W.G. - WELL GRADED
  - PEN - PENETRATION
  - G.W.T. - GROUND WATER TABLE
  - B.P.F. - BLOWS PER FOOT.

**UTAH STATE DEPARTMENT OF HIGHWAYS**  
SALT LAKE CITY, UTAH  
STRUCTURES DIVISION

HILLFIELD INTERCHANGE TO DAVIS-WEBER CD LINE  
HILLFIELD RD INTERCH O-PASS  
SOIL DATA

|                                   |                |                |
|-----------------------------------|----------------|----------------|
| DESIGNED BY MW                    | CHECKED BY MJG | I-15-7(58) 332 |
| DRAWN BY PAULSON                  | CHECKED BY     | PROJECT NUMBER |
| QUANTITIES BY                     | CHECKED BY     | 72916272 I-15  |
| APPROVAL RECOMMENDED BY M. J. ... | DATE           | STATION        |
| APPROVED July 27 1963             | DATE           | DAVIS          |
|                                   |                | COUNTY         |

| NO. | BY | DATE | REMARKS |
|-----|----|------|---------|
|     |    |      |         |

DRG NO. D-719 3 of 13

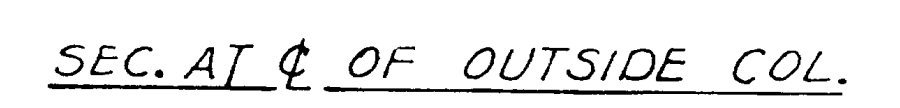
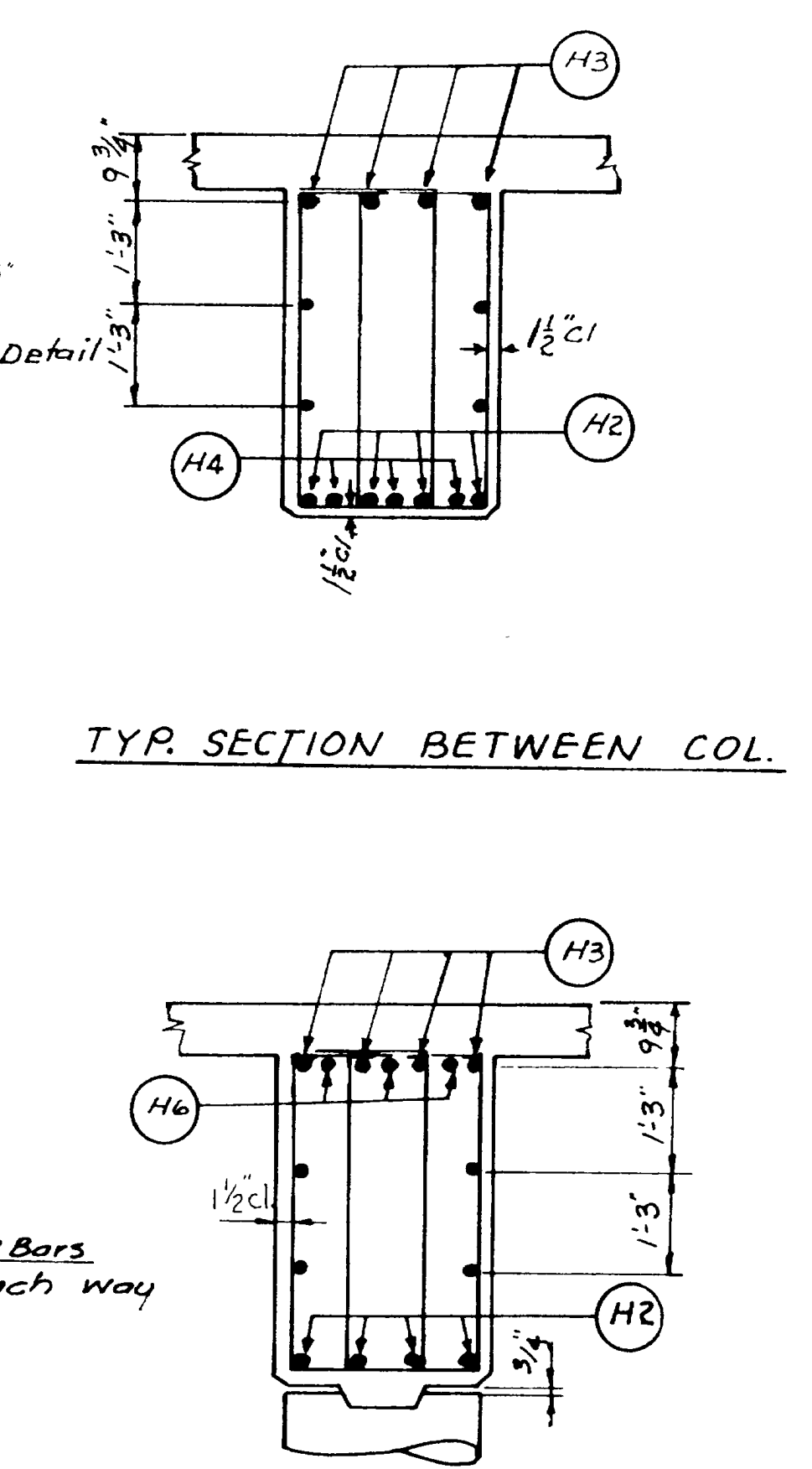
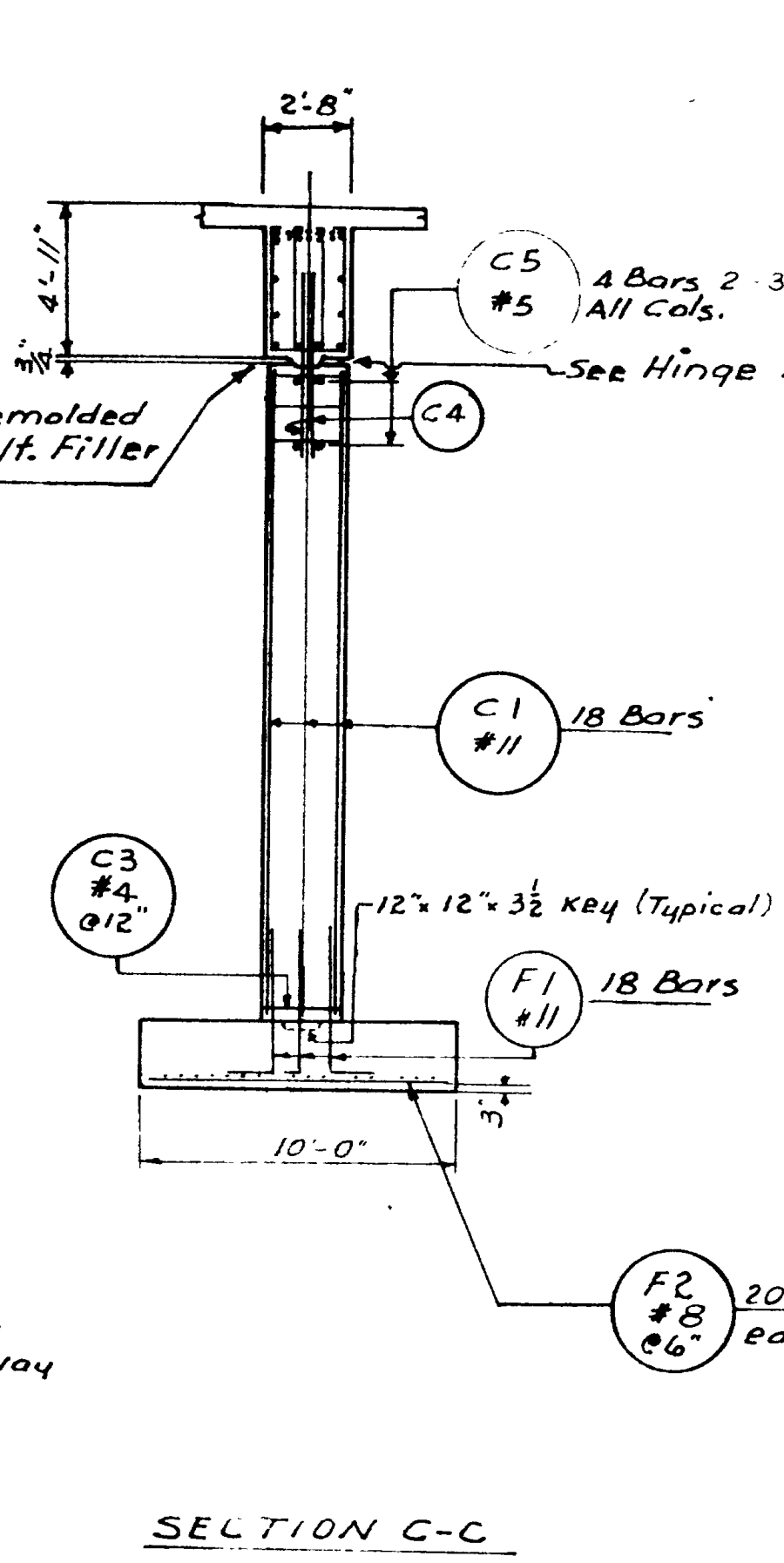
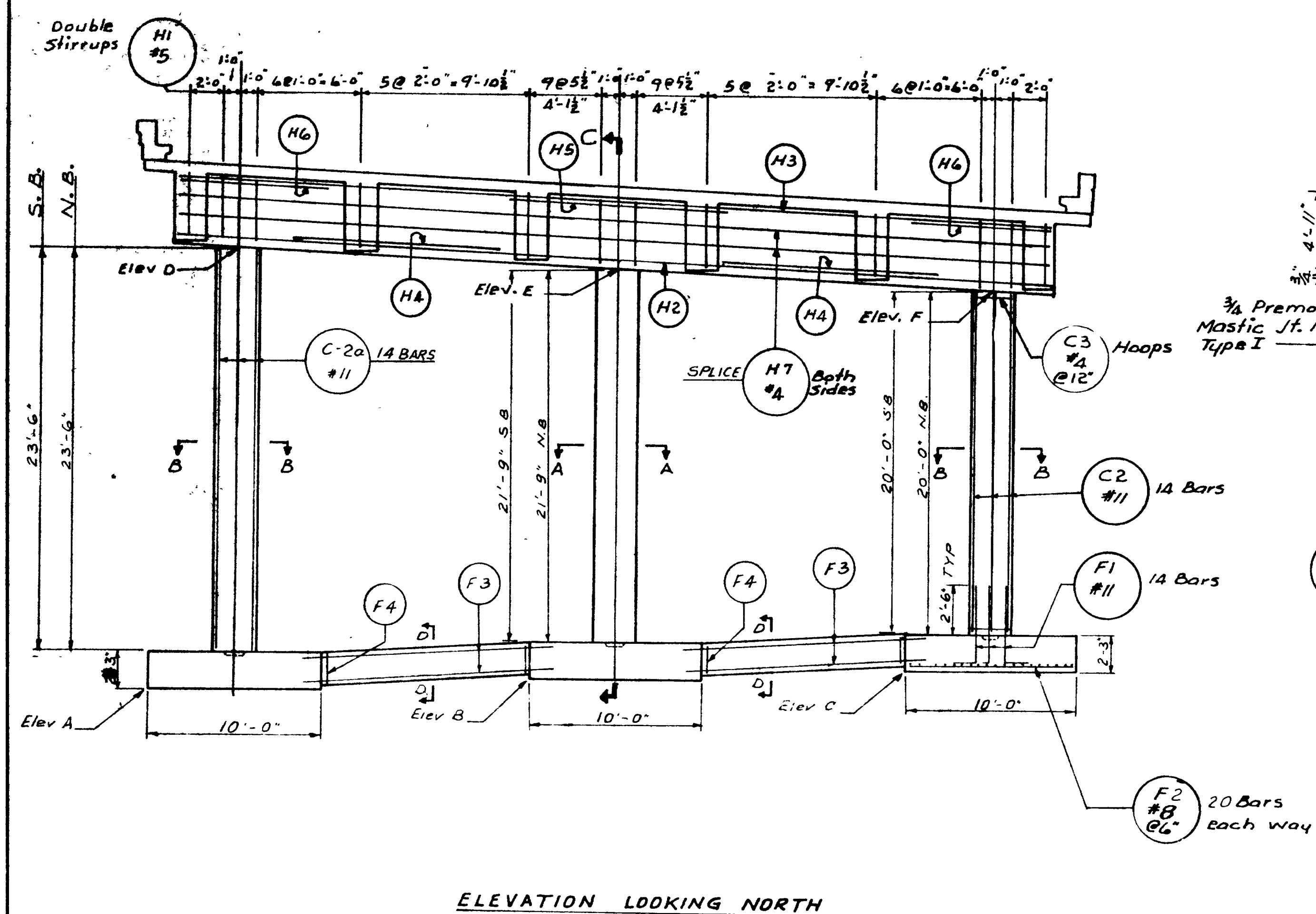
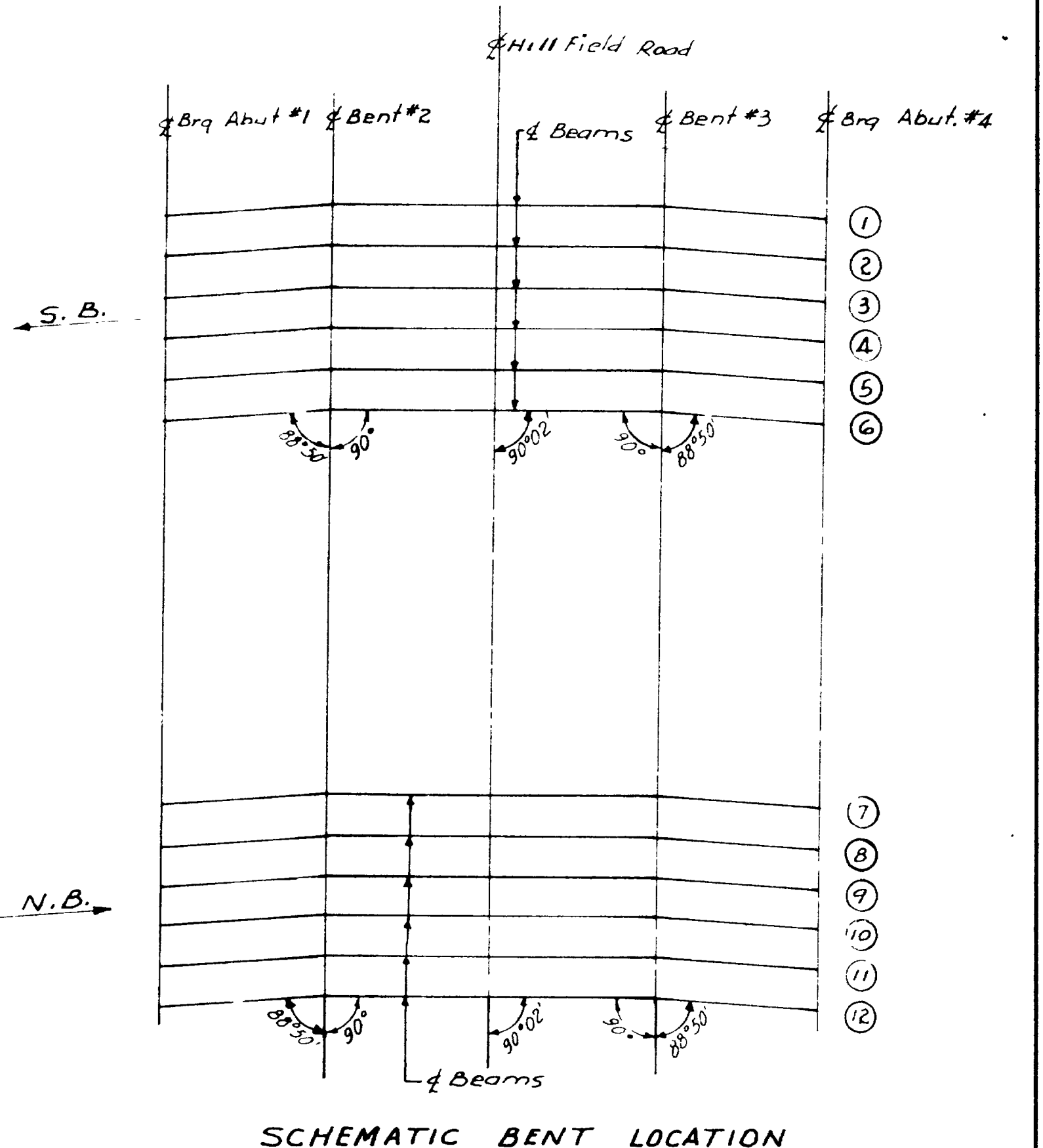
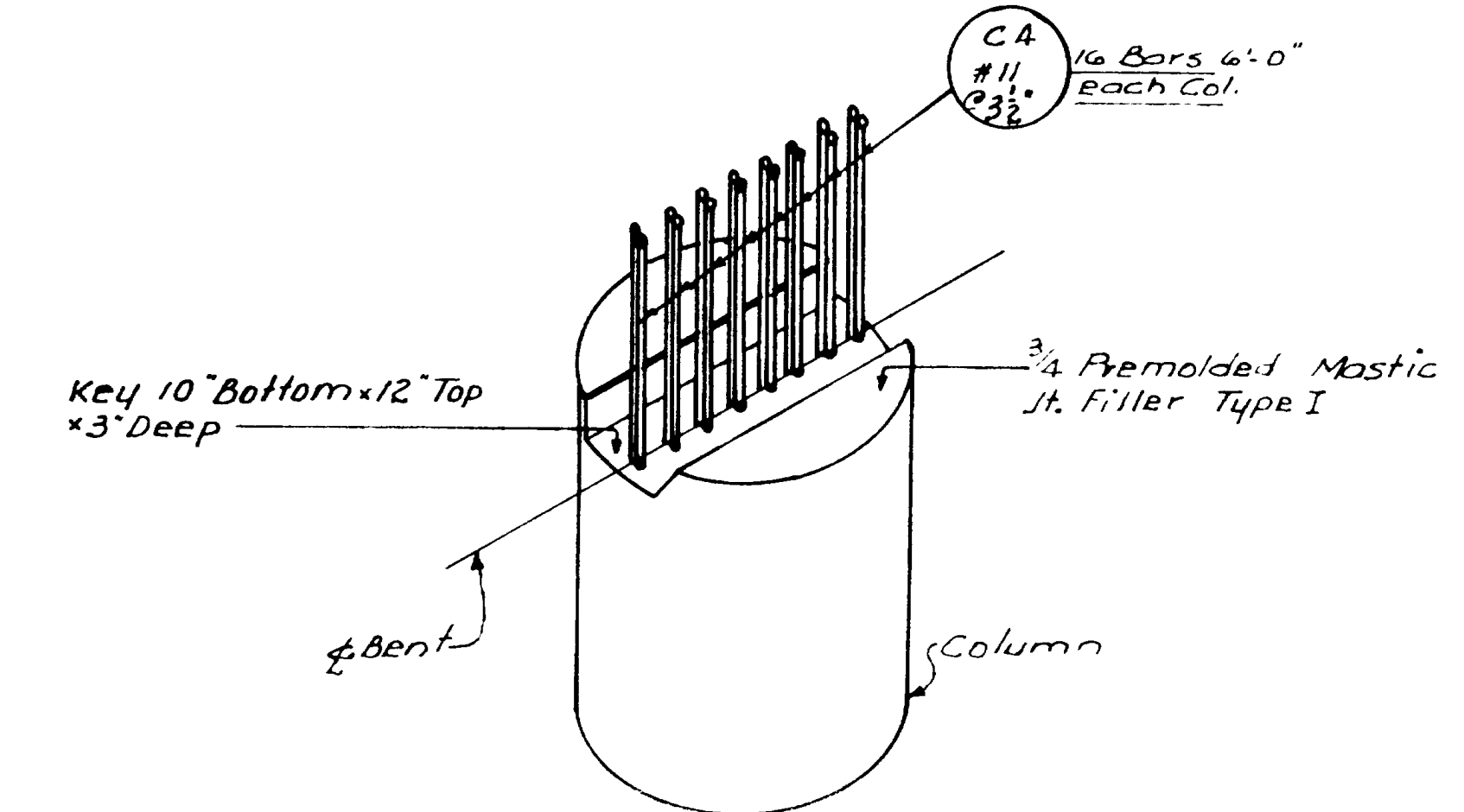
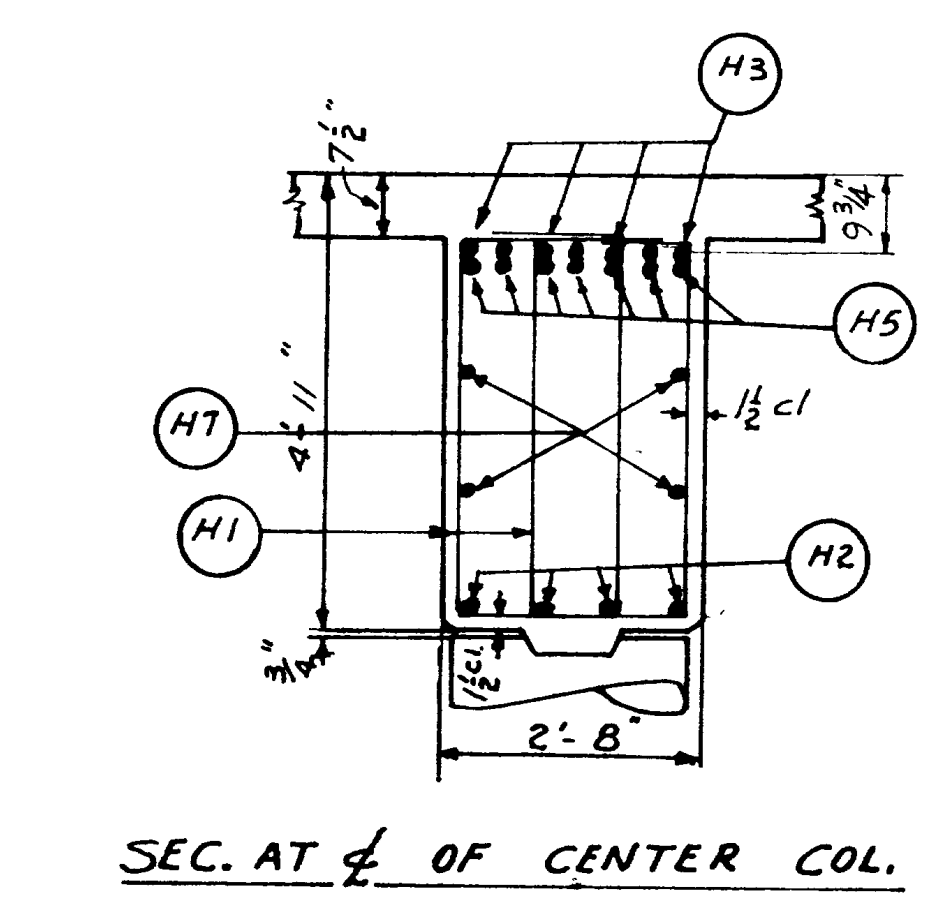
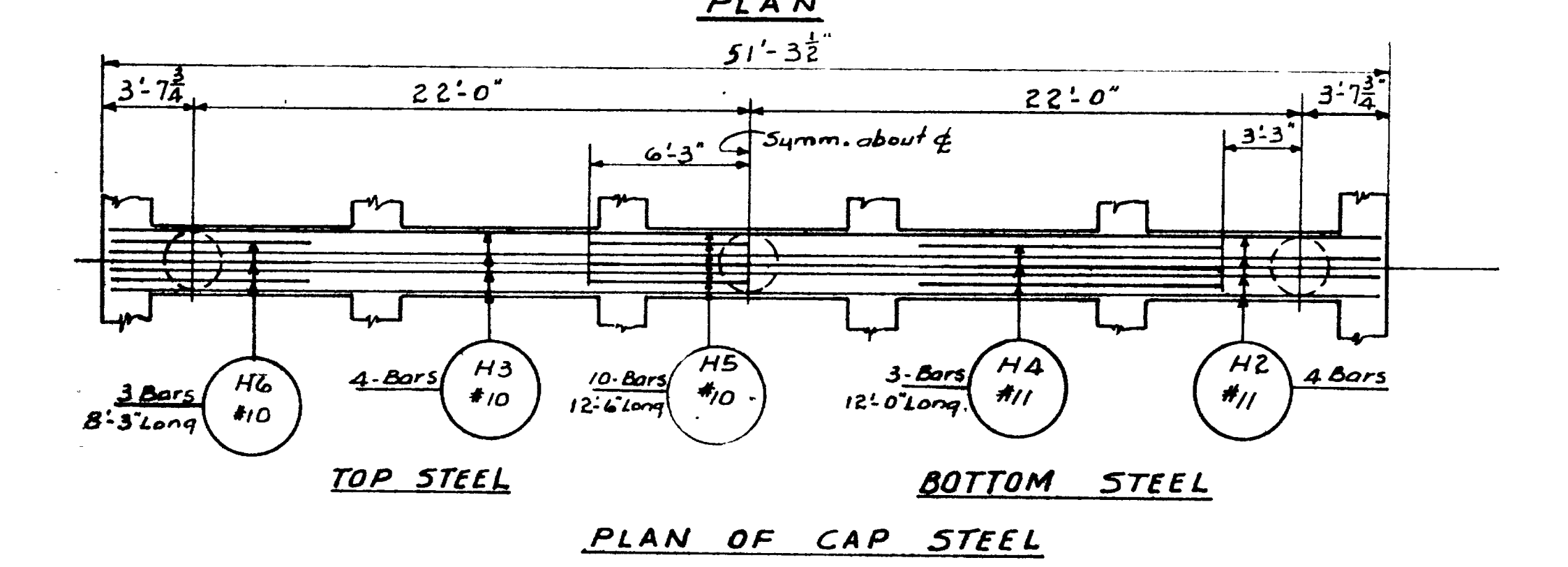
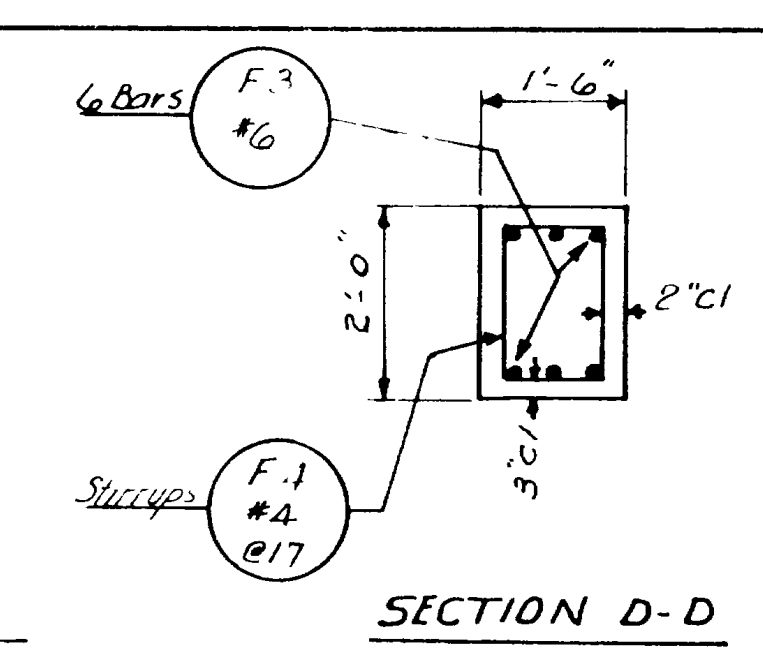
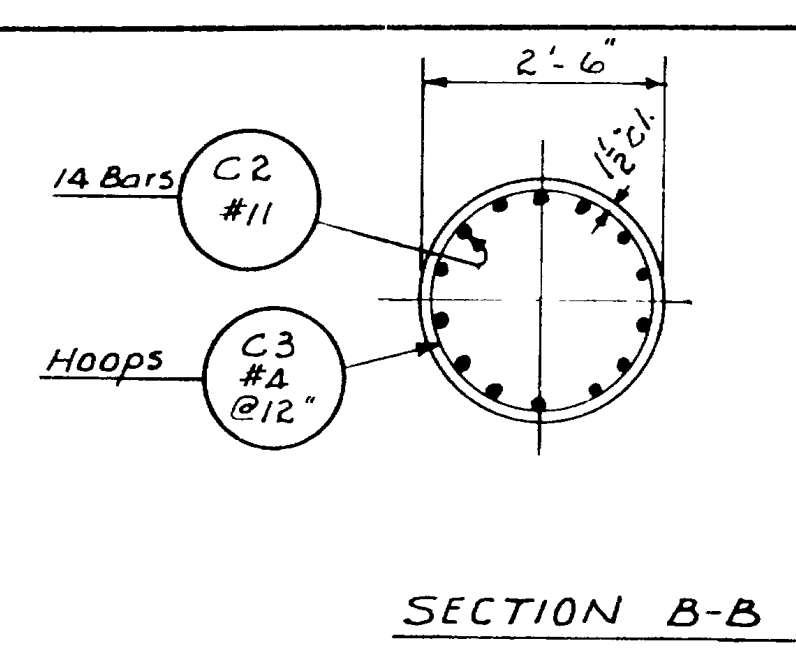
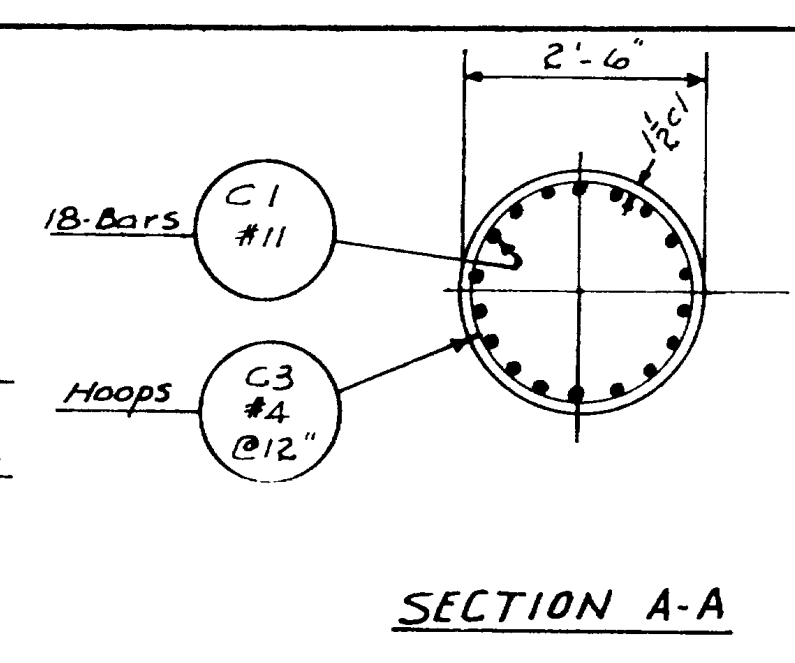
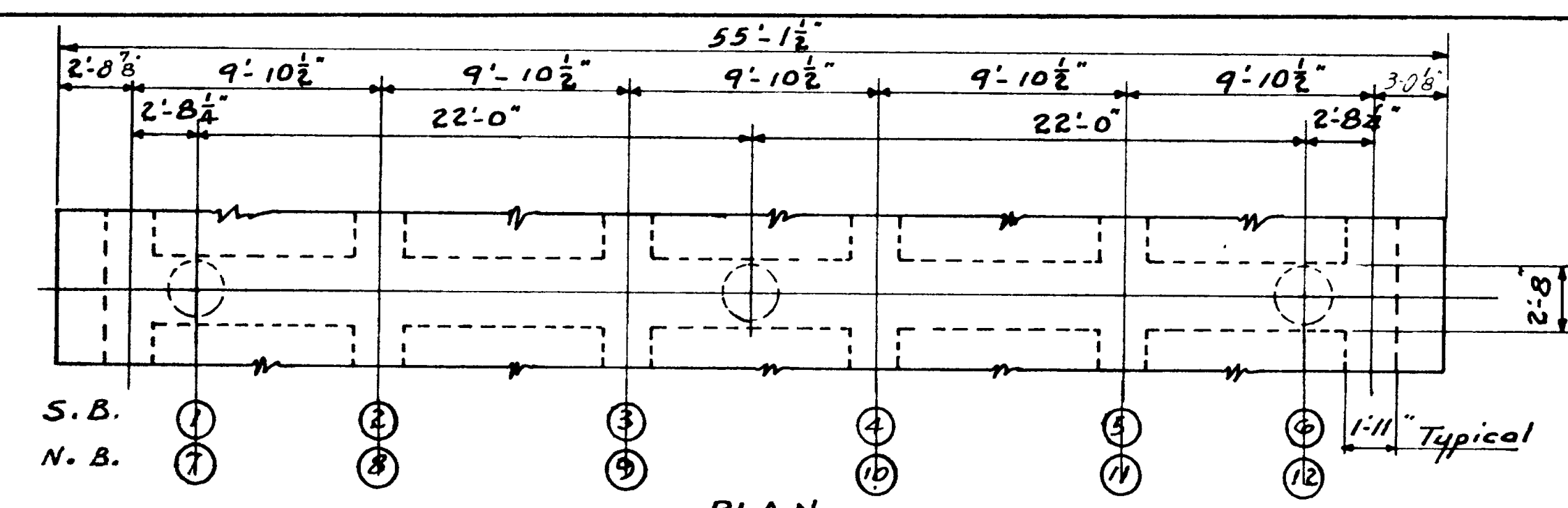








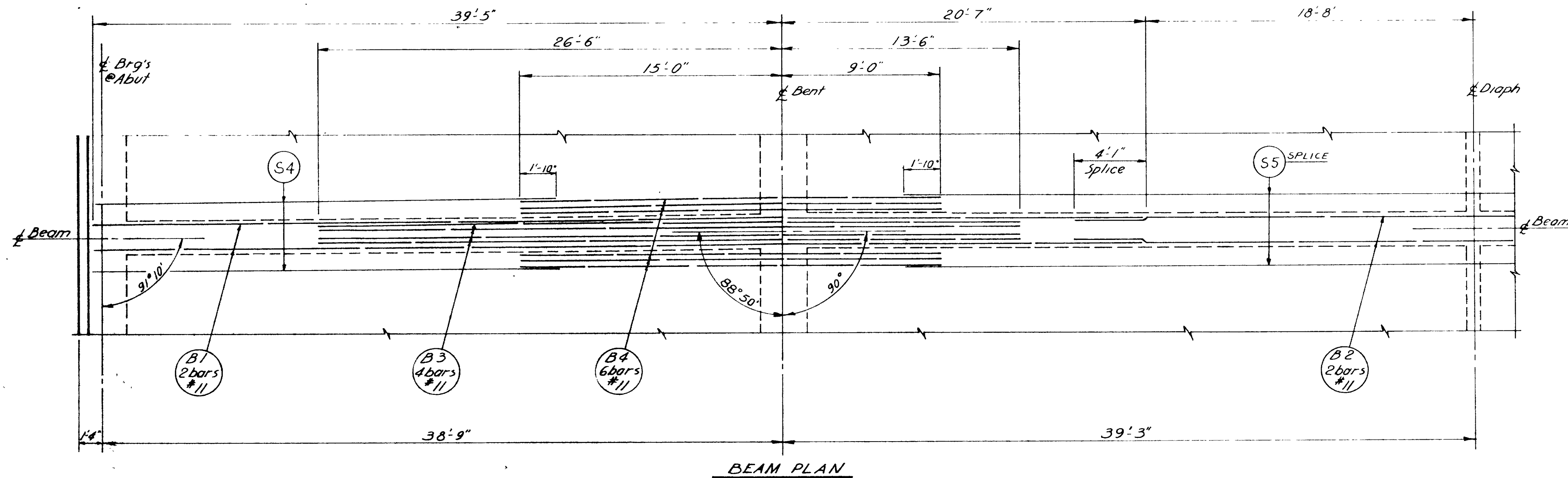
| TABLE OF ELEVATIONS |        |        |        |        |        |        |
|---------------------|--------|--------|--------|--------|--------|--------|
| POINT               | A      | B      | C      | D      | E      | F      |
| BENT No. 2 S.B.     | 456464 | 456529 | 456544 | 459039 | 458929 | 458819 |
| BENT No. 3 S.B.     | 456437 | 456502 | 456517 | 459012 | 458902 | 458792 |
| BENT No. 2 N.B.     | 456735 | 456800 | 456815 | 459370 | 459260 | 459150 |
| BENT No. 3 N.B.     | 456762 | 456827 | 456842 | 459337 | 459227 | 459117 |



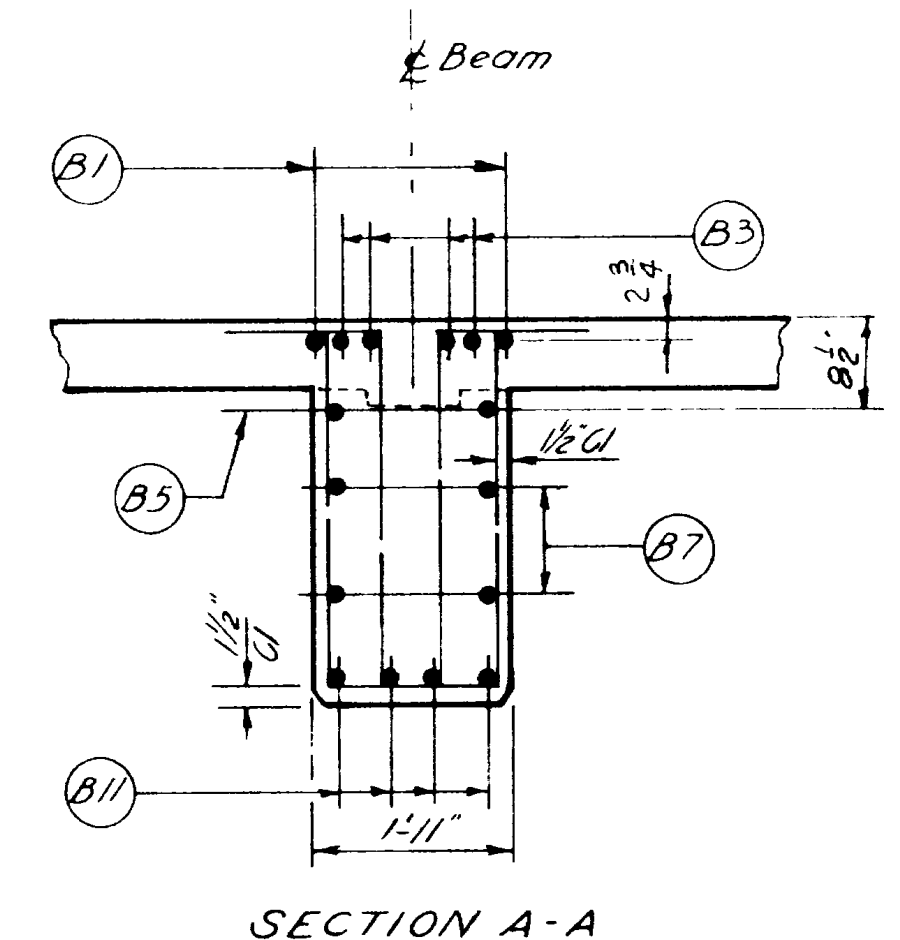
NOTE:  
Elevations D, E, F, at Bottom of 3/4\"/>

|  |                            |                |  |
|--|----------------------------|----------------|--|
| UTAH STATE DEPARTMENT OF HIGHWAYS<br>SALE EARL CITY, UTAH<br>STRUCTURES DIVISION                   |                            |                |  |
| HILLFIELD INTERCHANGE TO DAVIS-WEBER CO. LINE<br>HILL FIELD RD. INTERCHANGE O'PASS<br>BENT DETAILS |                            |                |  |
| DESIGNED BY: M.W.  | CHECKED BY: M.F.L.         | I-15-7(58)332  |  |
| DRAWN BY: B.M.W.   | BY: H.J.S.                 | PROJECT NUMBER |  |
| QUANTITIES BY: H.J.S.  | BY: B.M.W.                 | 729+62.72      |  |
| APPROVAL:  | RECOMMENDED BY: R.W. ALLEN | STATION        |  |
| APPROVAL:  | DATE:                      | Davis          |  |
| COUNTY:  |                            | COUNTY:        |  |

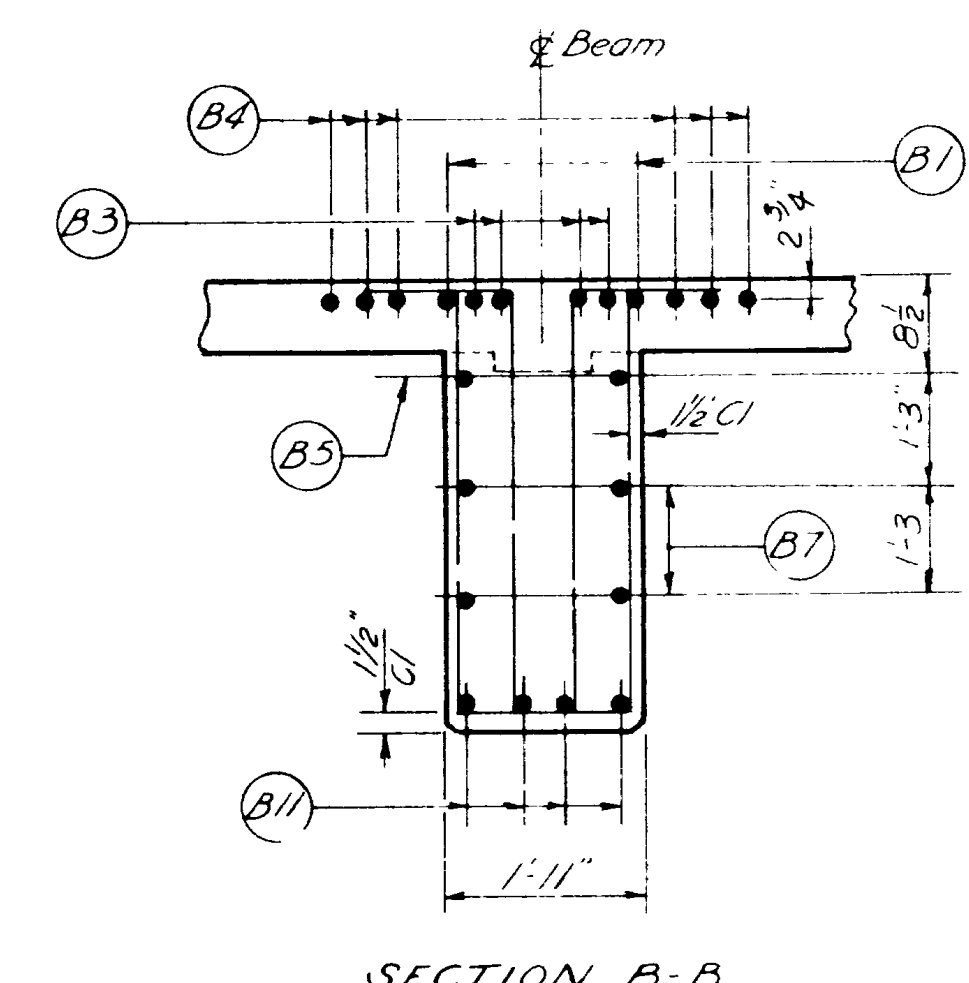
| NO.       | BY | DATE | REMARKS |
|-----------|----|------|---------|
| REVISIONS |    |      |         |



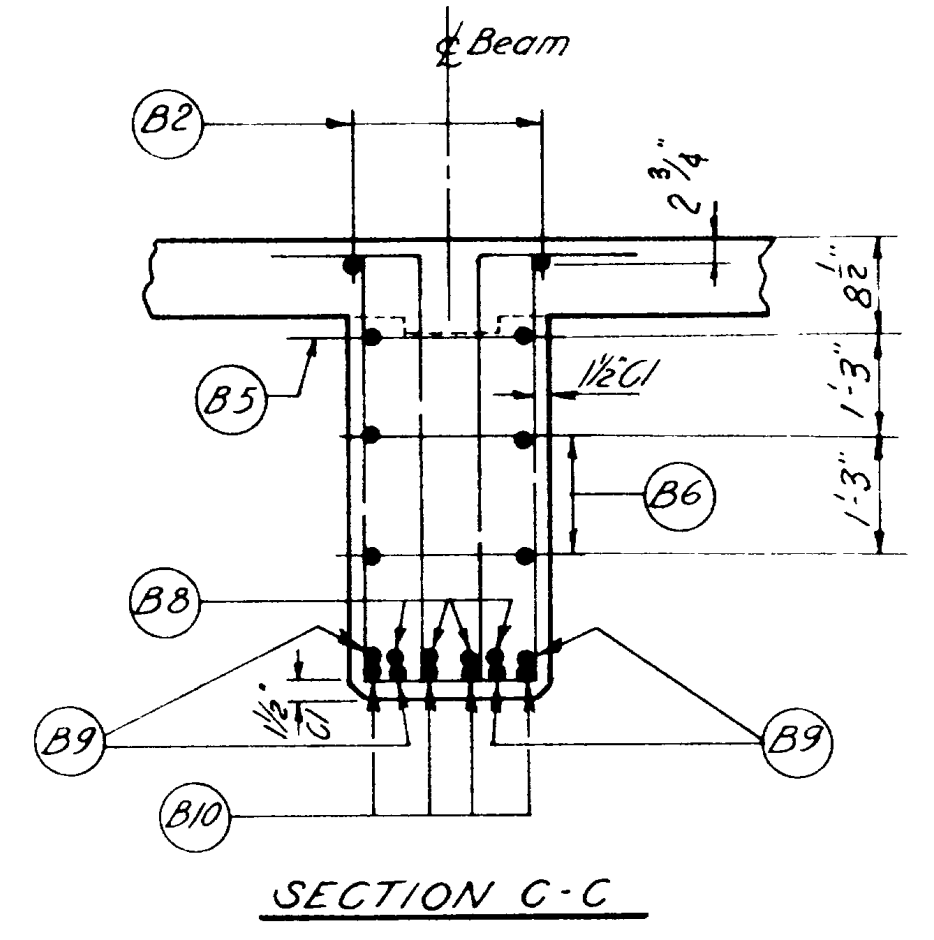
BEAM PLAN



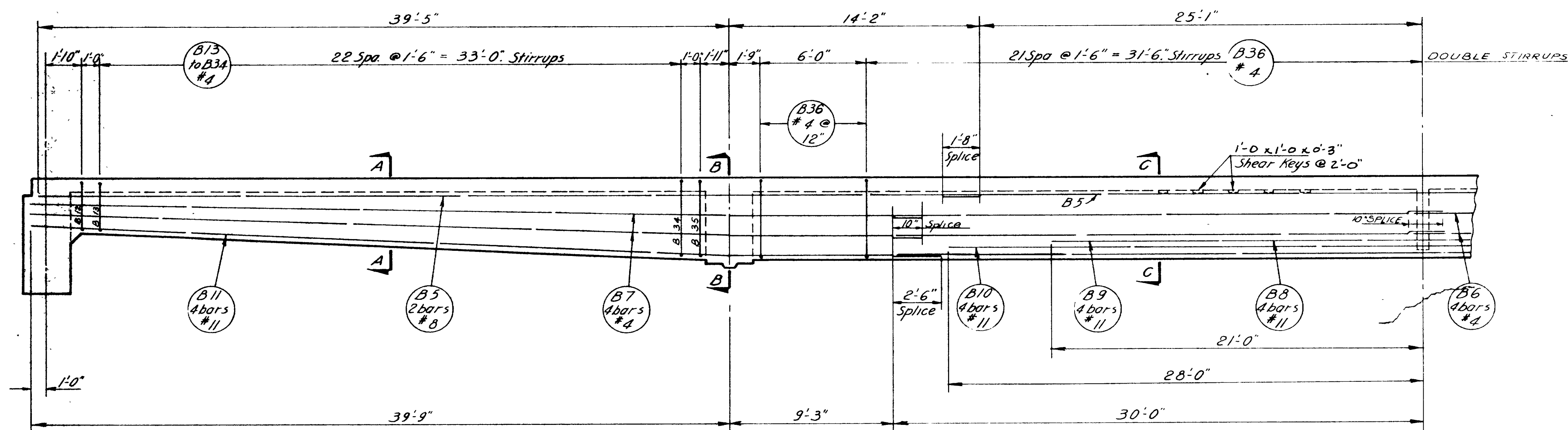
SECTION A-A



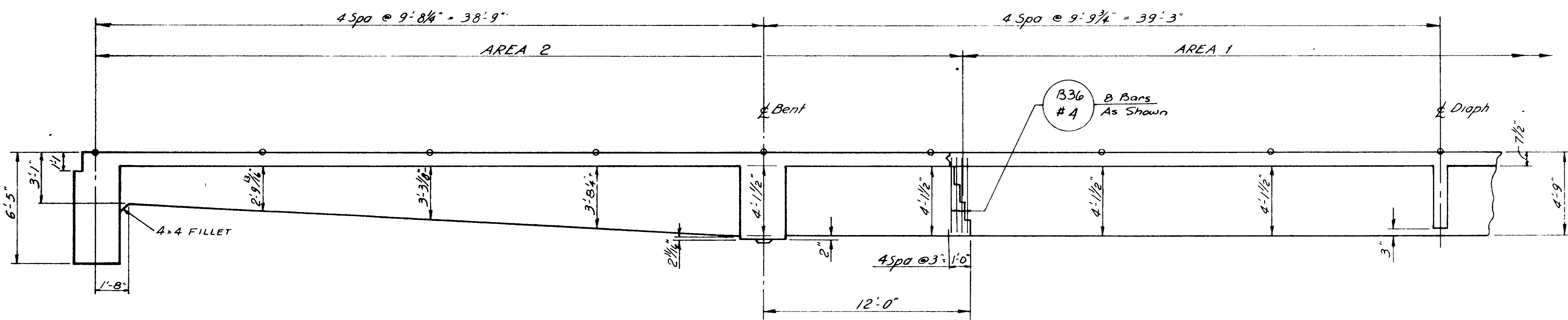
SECTION B-B



SECTION C-C



SECTIONAL ELEVATION



BEAM DEPTH AT E OF BEAM AND CONCRETE PLACING SEQUENCE

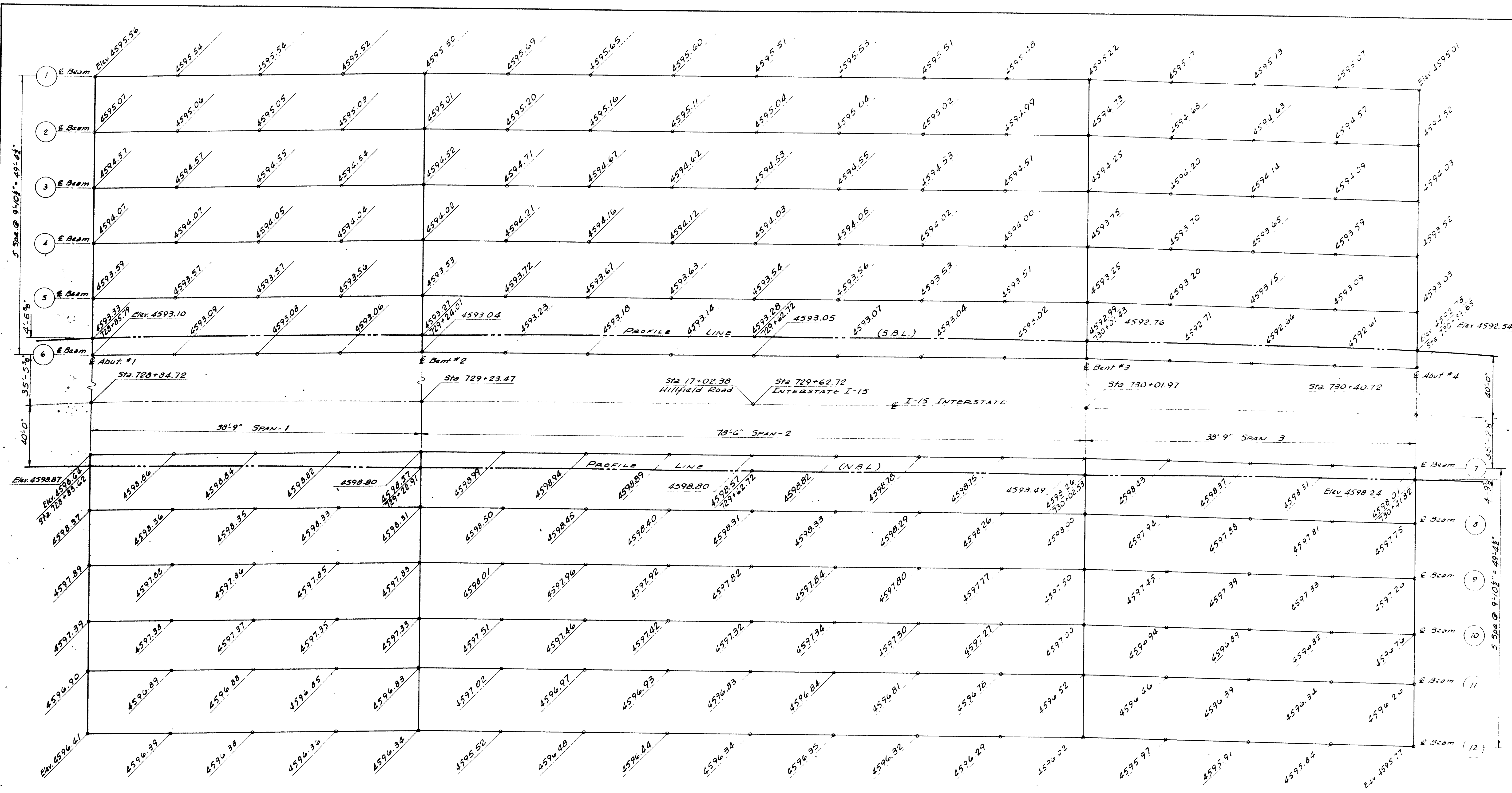
NOTE:  
 AREA 1 and AREA 2 indicate sequence of placing beam stem concrete. AREA 2 may be placed simultaneously with AREA 1 when approved by the Engineer and provided that the AREA 1 sections are placed in adjoining spans.  
 Shear Keys shall be provided when concrete deck is not placed simultaneously with beam stems.

|   |                    |                           |  |
|---|--------------------|---------------------------|--|
| UTAH STATE DEPARTMENT OF HIGHWAYS             |                    |                           |  |
| STRUCTURES DIVISION                           |                    |                           |  |
| HILLFIELD INTERCHANGE TO DAVIS-WEBER CO. LINE |                    |                           |  |
| HILL FIELD ROAD INTERCHANGE O'PASS            |                    |                           |  |
| BEAM DETAILS                                  |                    |                           |  |
| DESIGNED BY: M.W.                             | CHECKED BY: M.J.C. | PROJECT NO: I-15-7(58)332 |  |
| DRAWN BY: H.D.S.                              | CHECKED BY: H.J.S. | PROJECT MBER              |  |
| QUANTIFIED BY: H.J.S.                         | BY: B.M.W.         | 729162 72                 |  |
| APPROVAL                                      | STATION            | DAVIS                     |  |
| APPROVAL                                      | DATE               | DATE                      |  |

| NO. | BY | DATE | REVISIONS | REMARKS |
|-----|----|------|-----------|---------|
|     |    |      |           |         |



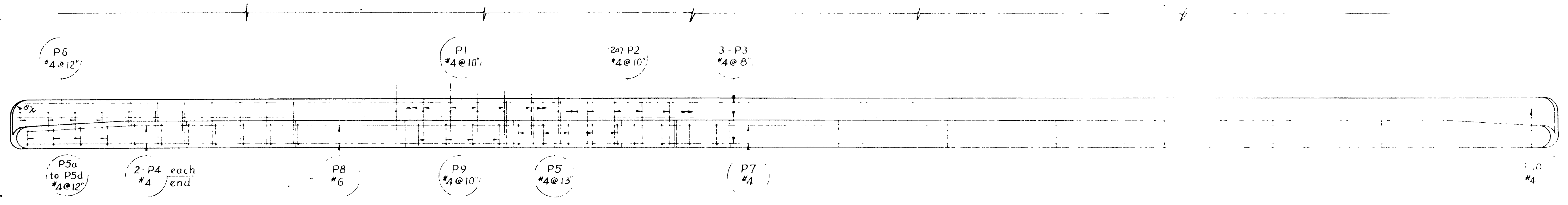




NOTE  
 Elevations shown are finished grade plus  
 1.25 x total dead load deflection

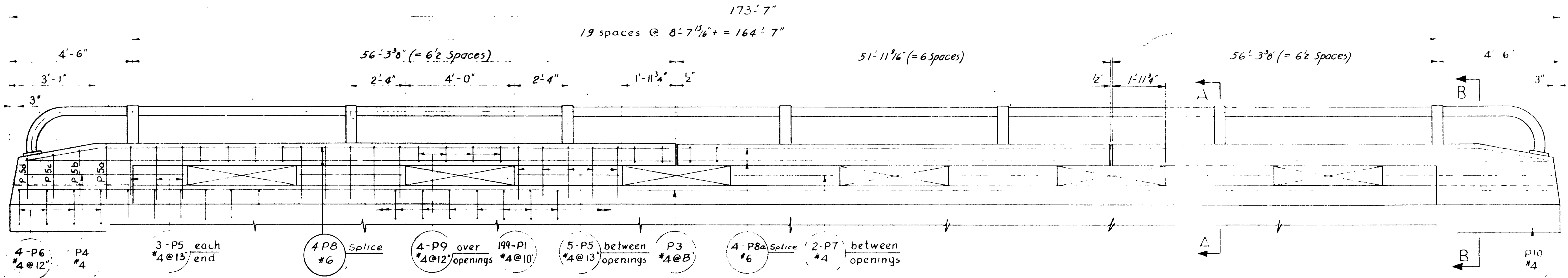
|  |                   |          |               |
|--|-------------------|----------|---------------|
| UTAH STATE DEPARTMENT OF HIGHWAYS            |                   |          |               |
| STRUCTURES DIVISION                          |                   |          |               |
| HILLFIELD INTERCHANGE TO DAVIS WEBER CO LINE |                   |          |               |
| HILL FIELD 2D INTERCH O'PASS                 |                   |          |               |
| SCREED ELEVATIONS                            |                   |          |               |
| DESIGNED BY                                  | MW                | DATE     | 11-15-78      |
| CHECKED BY                                   | BE                | DATE     | 11-15-78      |
| PROJECT NO.                                  | HJS               | STATION  | 729+62.72     |
| CONTRACT NO.                                 | 15-7(38)332       | LOCATION | DAVIS         |
| BY   | M. J. Christensen | DATE     | 11-15-78      |
| REVISIONS                                    |                   |          |               |
| NO.  | DATE              | REMARKS  |               |
| 1  |                   |          |               |
|  |                   |          | D-719 9 of 13 |





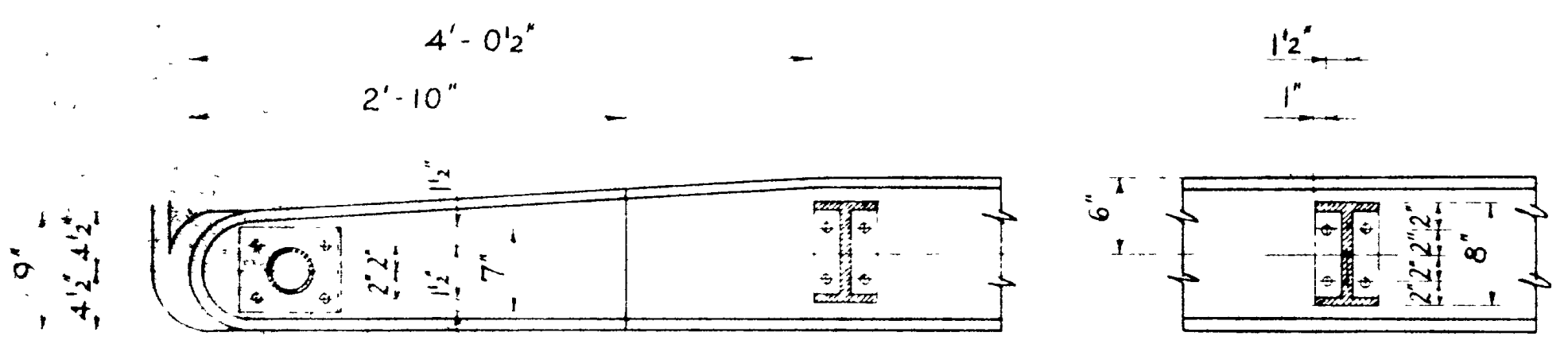
PARAPET PLAN

NOTE  
 1. The curb and parapet shall not be placed until at least 7 days after the slab has been poured and until all shoring has been removed.  
 2. See Sheet MR 101 for Details of Rail Installation.

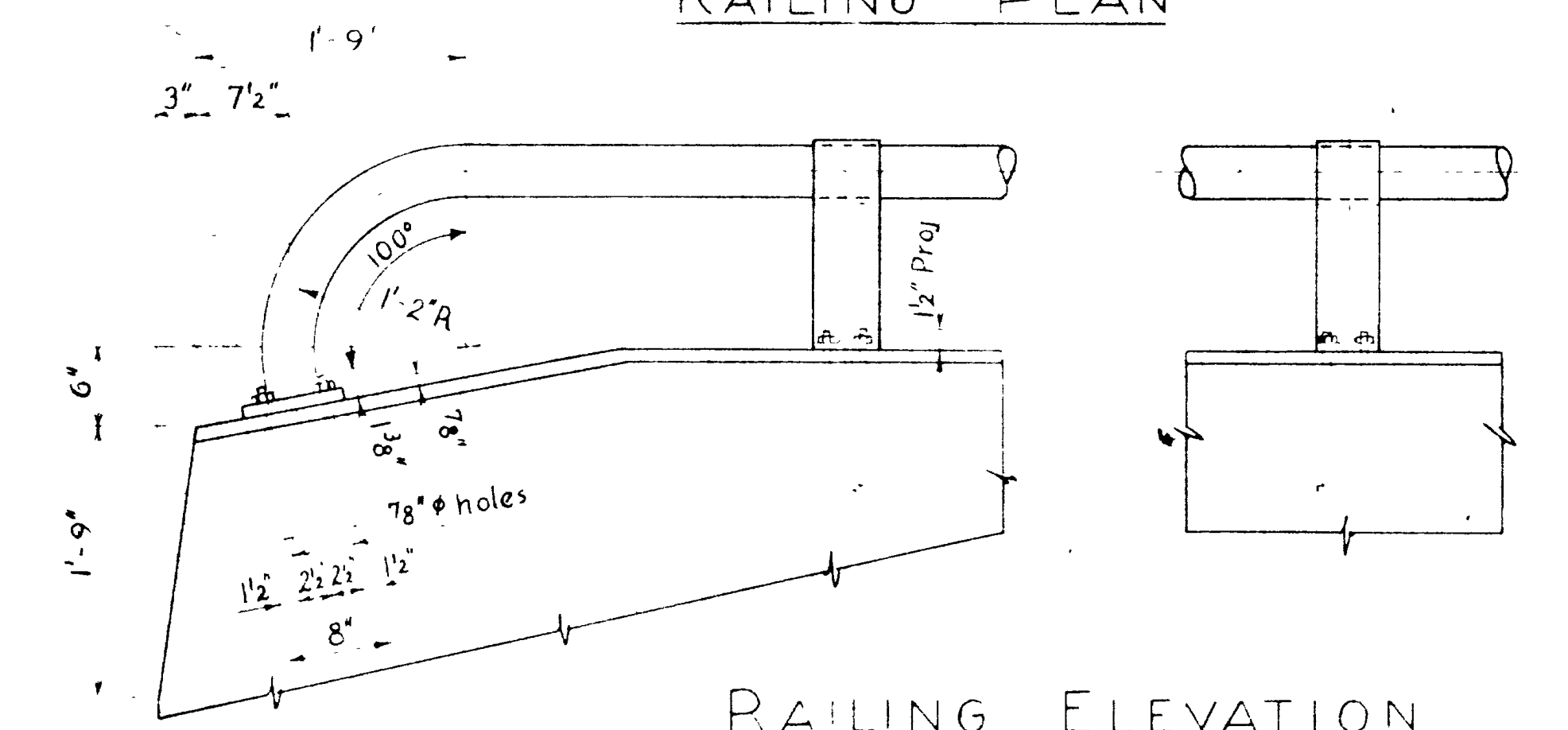


PARAPET ELEVATION

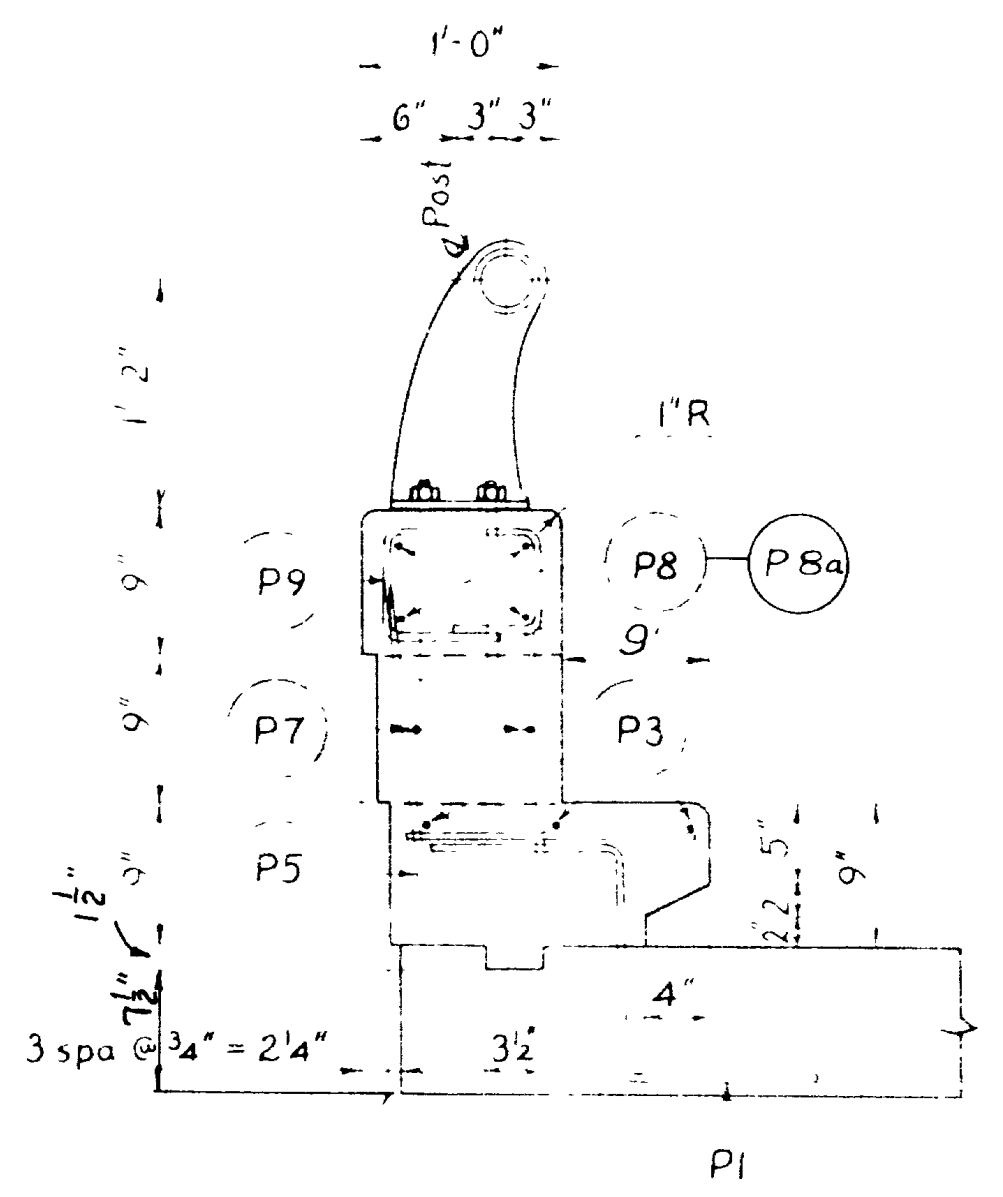
Continue rail parapet details to of parapet until the curb is reached



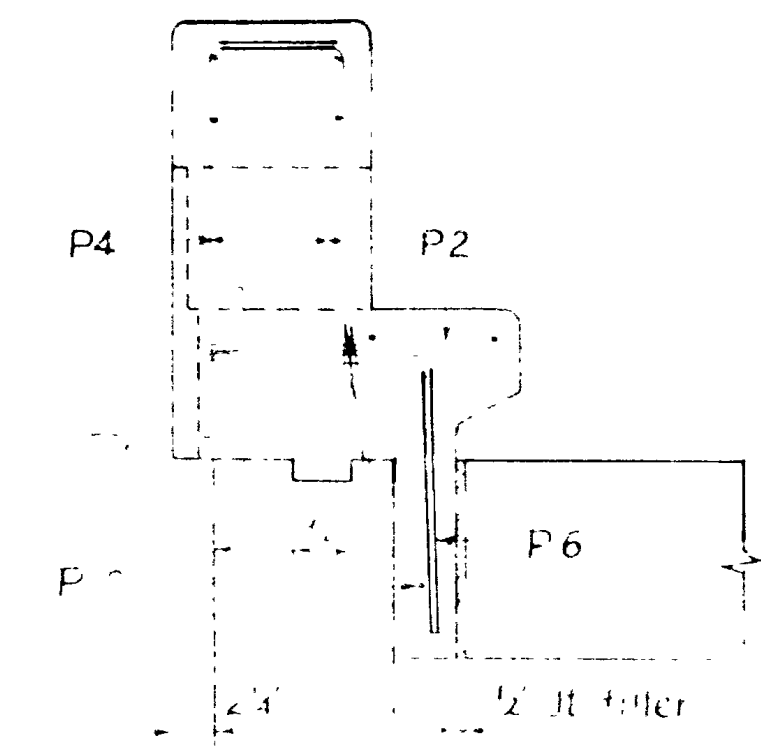
RAILING PLAN



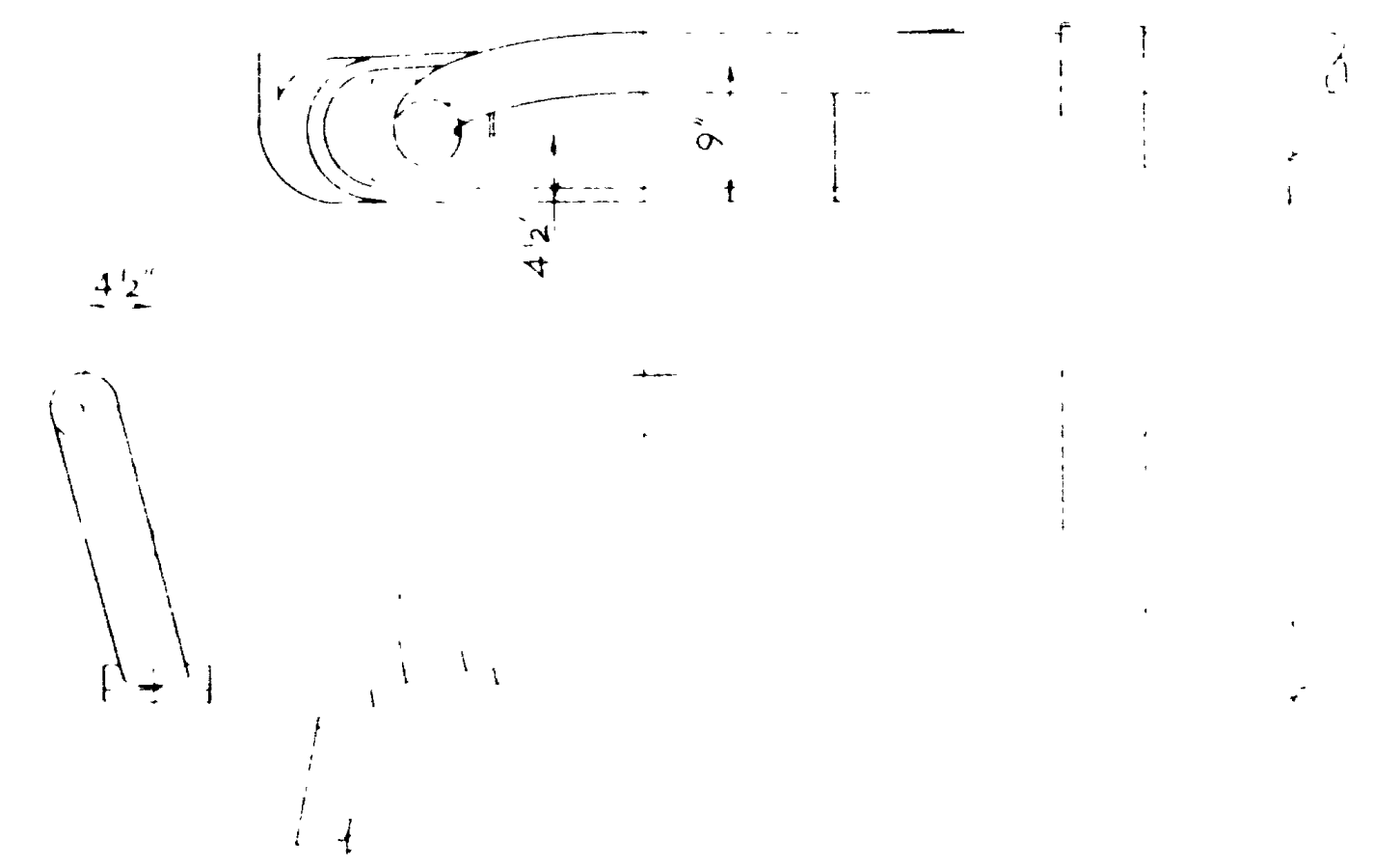
RAILING ELEVATION



SECTION A-A



SECTION B-B



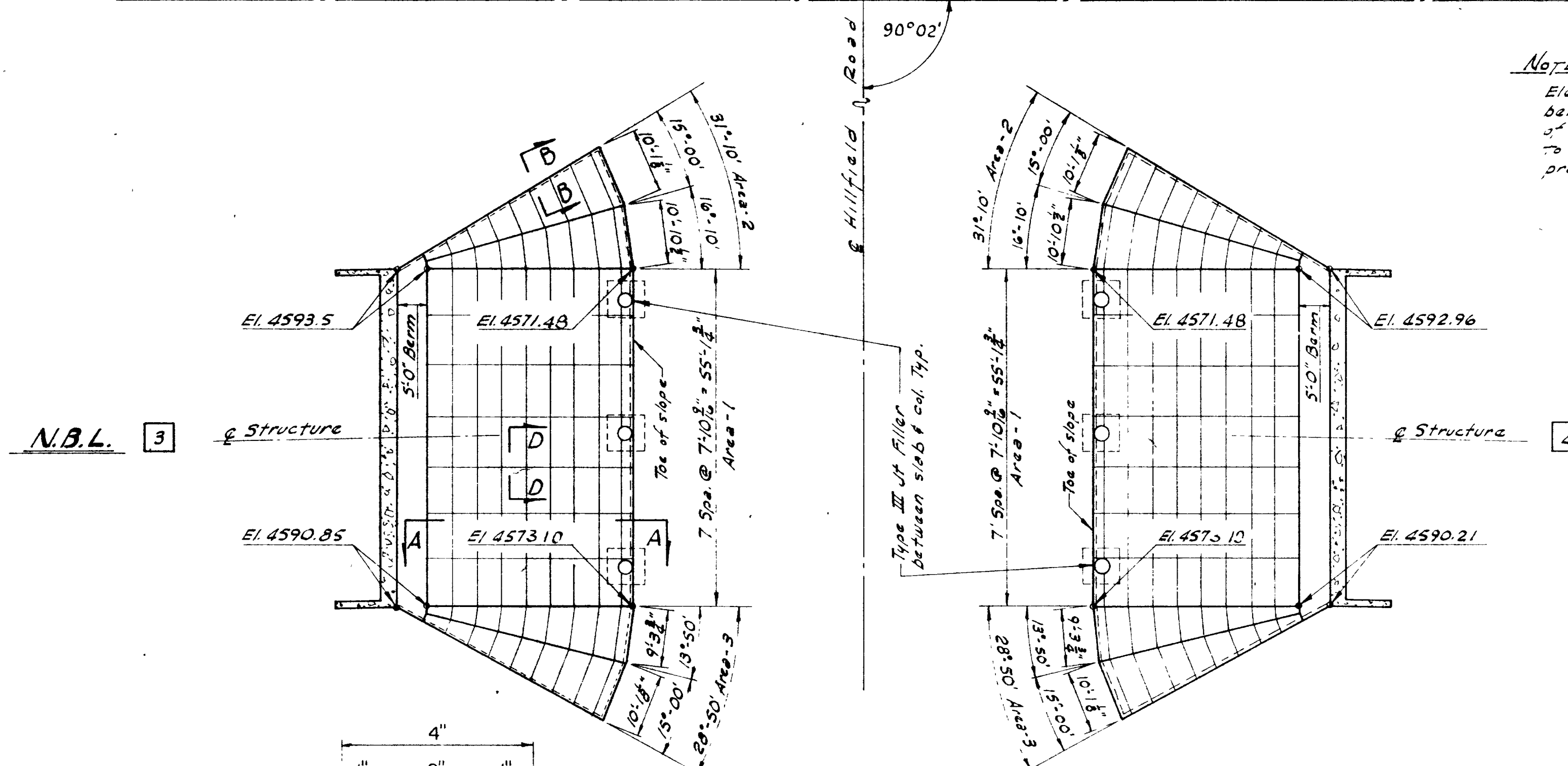
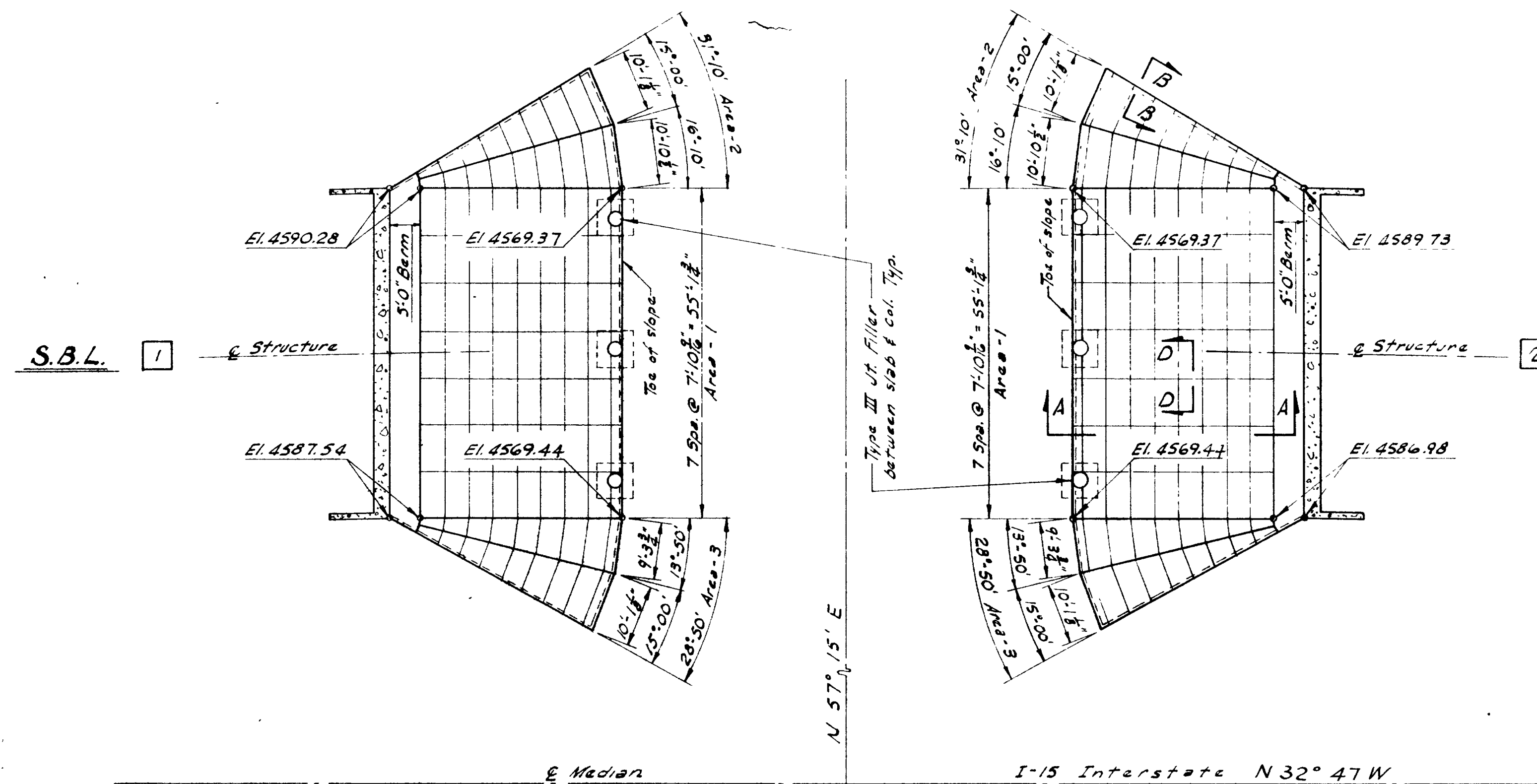
DETAIL A-A

STATE OF CALIFORNIA  
 HILLFIELD INTERCHANGE TO DAVIS WEBER CO LINE  
 HILLFIELD, CALIFORNIA  
 M.W. H.J.S. I-15-7(5B)332  
 H.C.S. BMW 129+62.72  
 DAVIS  
 D-719 10 13

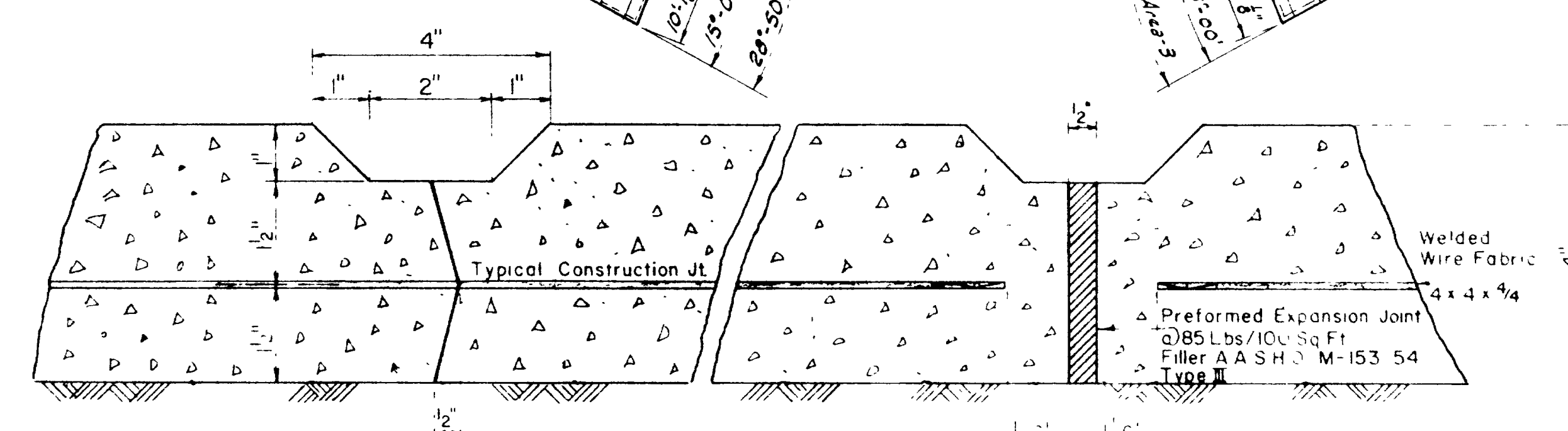
| CONC SLOPE PROTECTION |            |          |          |          |
|-----------------------|------------|----------|----------|----------|
| Location No           | Avg Slope  | Area     | Quantity | Quantity |
| 1                     | 34°-7 1/2' | 2,127.72 | 892.25   | 334.91   |
| 2                     | 35°-7 1/2' | 2,127.72 | 872.28   | 332.43   |
| 3                     | 32°-12'    | 2,127.85 | 890.51   | 330.49   |
| 4                     | 38°-4 1/2' | 2,127.55 | 874.51   | 333.35   |
|                       |            |          | 1337.03  |          |

NOTES

- ① Quantity in sq Yd. to be placed on surface
- ② Area 1' - BERM WIDTH x LENGTH x SLOPE LENGTH
- ③ Area 2' or 3' - BERM WIDTH x LENGTH x SLOPE LENGTH
- ④ Elevations at the toe of slope are approximate only and shall be verified in the field by the Engineer at time of construction.

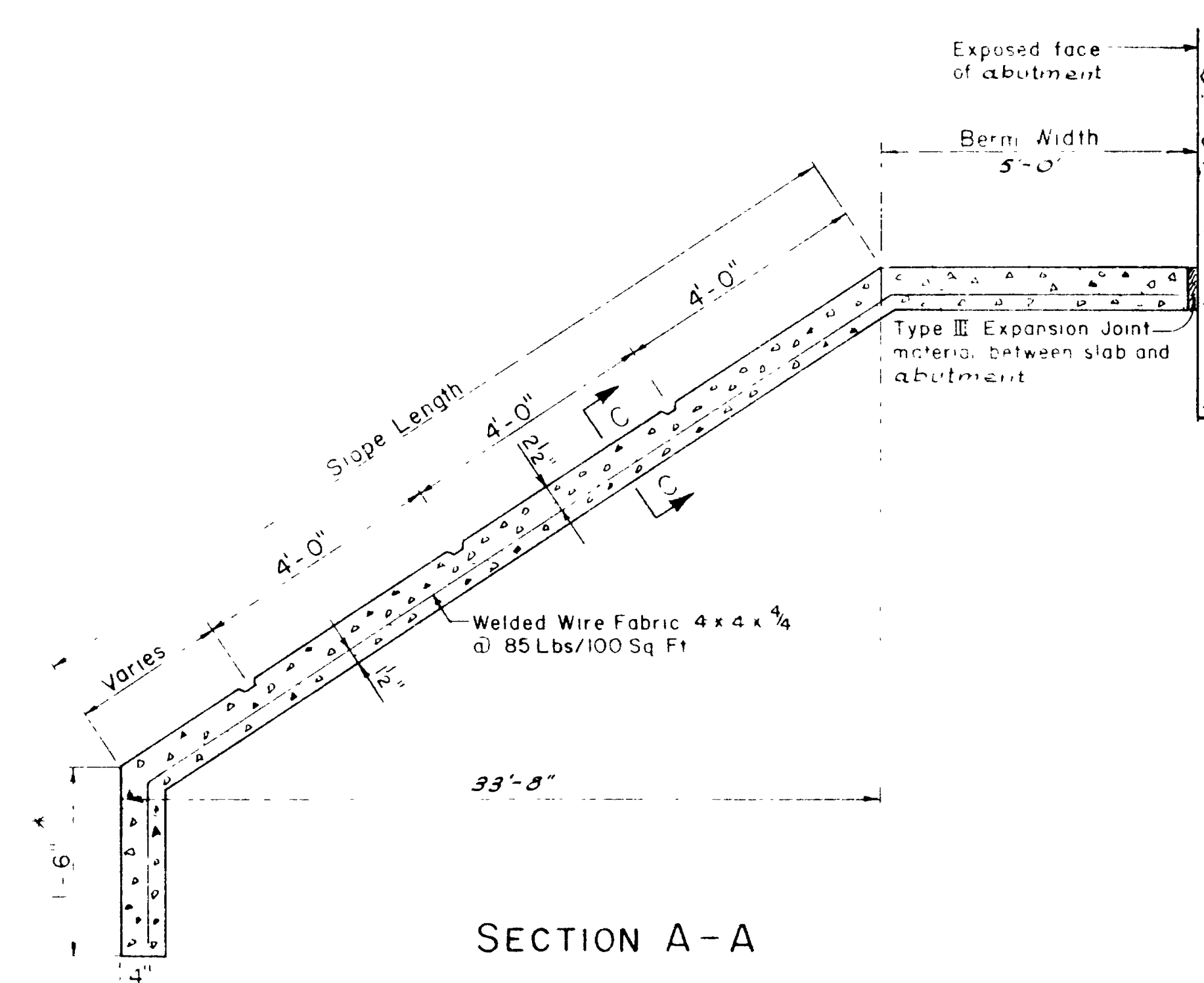
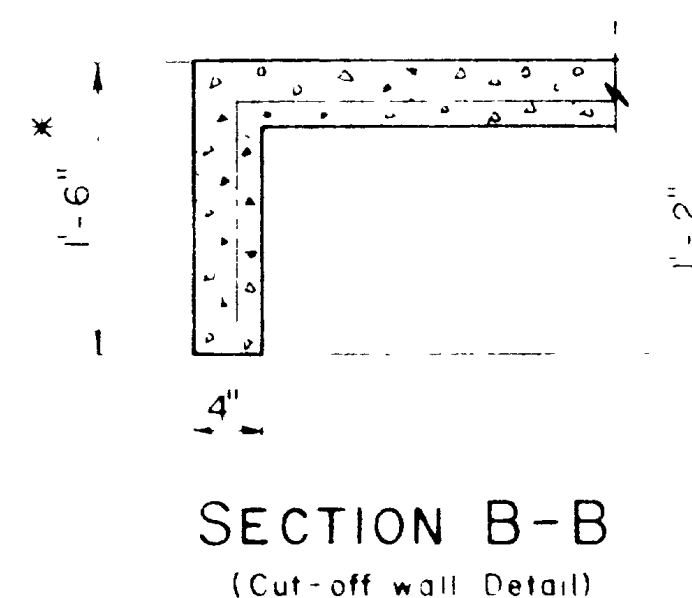


**Notes:**  
Elevations shown @ front edge of berm, toe of slope and front face of bridge abutment are given to top of concrete slope protection.



Locate Construction Joints as needed in placing concrete slab. Locate vertical Expansion Joints at 40'-0" max spacing in slab.

**SECTION C-C**                      **SECTION D-D**



**NOTE**  
Cut-off Wall is not included in exposed finished surface

UTAH STATE DEPARTMENT OF HIGHWAYS  
STRUCTURES DIVISION  
HILLFIELD INTERCHANGE TO DAVIS-WEBER CO LINE  
HILLFIELD RD INTERCH. O.D.S.  
REIN. CONC SLOPE PROTECTION

MW H.J.C. 2-15-7(58)932  
HGE/HEP H.J.S. 729-0272  
H.J.S. 211/5  
mfg. Christianen  
2-13

0-7,9 11-13



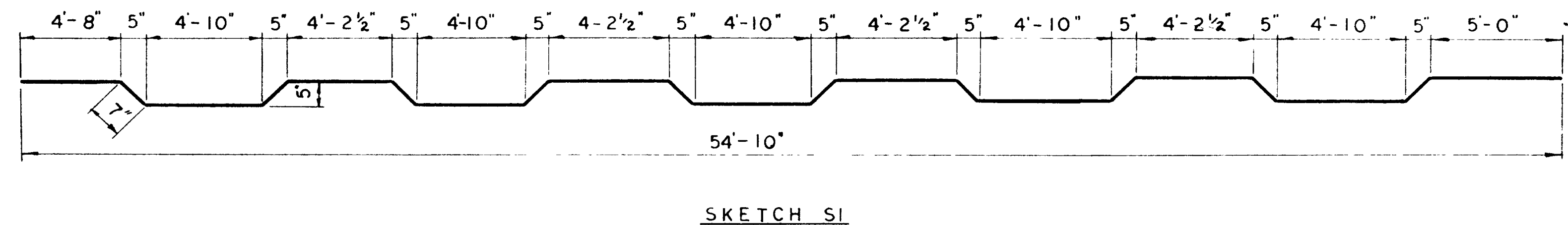
| MARK | LOCATION      | SIZE NO | LENGTH  | NO BARS | TOTAL LENGTH | SKETCH                              |
|------|---------------|---------|---------|---------|--------------|-------------------------------------|
| A1   | ABUTMENTS     | 4       | 17'-11" | 128     | 2293'-4"     |                                     |
| A2   |               | 4       | 27'-10" | 48      | 1336'-0"     |                                     |
| A3   |               | 6       | 28'-6"  | 64      | 1824'-0"     |                                     |
| A4   |               | 5       | 3'-0"   | 144     | 432'-0"      |                                     |
| A5   | ABUTMENTS     | 4       | 11'-6"  | 48      | 552'-0"      |                                     |
| AS1  | APPROACH SLAB | 4       | 17'-10" | 212     | 3780'-8"     |                                     |
| AS2  |               | 7       | 14'-8"  | 284     | 4165'-4"     |                                     |
| AS3  |               | 4       | 26'-10" | 256     | 6869'-4"     |                                     |
| AS4  | APPROACH SLAB | 7       | 27'-2"  | 24      | 652'-0"      |                                     |
| B1   | BEAMS         | 11      | 60'-0"  | 48      | 2880'-0"     |                                     |
| B2   |               | 11      | 45'-6"  | 24      | 1092'-0"     |                                     |
| B3   |               | 11      | 40'-0"  | 96      | 3840'-0"     |                                     |
| B4   |               | 11      | 24'-0"  | 144     | 3456'-0"     |                                     |
| B5   |               | 8       | 53'-7"  | 72      | 3858'-0"     |                                     |
| B6   |               | 4       | 50'-5"  | 96      | 2920'-0"     |                                     |
| B7   |               | 4       | 49'-10" | 96      | 4784'-0"     |                                     |
| B8   |               | 11      | 42'-0"  | 48      | 2016'-0"     |                                     |
| B9   |               | 11      | 42'-0"  | 48      | 2016'-0"     |                                     |
| B10  |               | 11      | 56'-0"  | 48      | 2688'-0"     |                                     |
| B11  |               | 11      | 51'-6"  | 96      | 4944'-0"     | BARS TO CONFORM WITH BOTTOM OF BEAM |
| B12  |               | 4       | 8'-3"   | 48      | 396'-0"      |                                     |
| B13  |               | 4       | 8'-5"   | 48      | 404'-0"      |                                     |
| B14  |               | 4       | 8'-7"   | 48      | 412'-0"      |                                     |
| B15  |               | 4       | 8'-7"   | 48      | 412'-0"      |                                     |
| B16  |               | 4       | 8'-9"   | 48      | 420'-0"      |                                     |
| B17  |               | 4       | 8'-11"  | 48      | 428'-0"      |                                     |
| B18  |               | 4       | 9'-1"   | 48      | 436'-0"      |                                     |
| B19  |               | 4       | 9'-3"   | 48      | 444'-0"      |                                     |
| B20  |               | 4       | 9'-3"   | 48      | 444'-0"      |                                     |
| B21  |               | 4       | 9'-5"   | 48      | 452'-0"      |                                     |
| B22  |               | 4       | 9'-7"   | 48      | 460'-0"      |                                     |
| B23  |               | 4       | 9'-9"   | 48      | 468'-0"      |                                     |
| B24  |               | 4       | 9'-11"  | 48      | 476'-0"      |                                     |
| B25  |               | 4       | 10'-1"  | 48      | 484'-0"      |                                     |
| B26  |               | 4       | 10'-3"  | 48      | 492'-0"      |                                     |
| B27  |               | 4       | 10'-3"  | 48      | 492'-0"      |                                     |
| B28  |               | 4       | 10'-5"  | 48      | 500'-0"      |                                     |
| B29  |               | 4       | 10'-7"  | 48      | 508'-0"      |                                     |
| B30  |               | 4       | 10'-9"  | 48      | 516'-0"      |                                     |
| B31  |               | 4       | 10'-11" | 48      | 524'-0"      |                                     |
| B32  |               | 4       | 10'-11" | 48      | 524'-0"      |                                     |
| B33  |               | 4       | 11'-1"  | 48      | 532'-0"      |                                     |
| B34  |               | 4       | 11'-3"  | 48      | 540'-0"      |                                     |
| B35  |               | 4       | 11'-5"  | 48      | 548'-0"      |                                     |
| B36  | BEAMS         | 4       | 11'-7"  | 1512    | 17514'-0"    |                                     |

| MARK | LOCATION   | SIZE NO | LENGTH  | NO BARS | TOTAL LENGTH | SKETCH |
|------|------------|---------|---------|---------|--------------|--------|
| C1   | COLUMNS    | 11      | 21'-7"  | 72      | 1554'-0"     |        |
| C2   |            | 11      | 19'-10" | 56      | 1110'-8"     |        |
| C2a  |            | 11      | 23'-4"  | 56      | 1306'-8"     |        |
| C3   |            | 4       | 7'-11"  | 268     | 2121'-8"     |        |
| C4   |            | 11      | 6'-0"   | 192     | 1152'-0"     |        |
| C5   | COLUMNS    | 5       | 2'-3"   | 48      | 108'-0"      |        |
| D1   | DIAPHRAGMS | 4       | 10'-5"  | 90      | 937'-6"      |        |
| D2   | DIAPHRAGMS | 5       | 26'-5"  | 16      | 422'-8"      |        |
| F1   | FOOTINGS   | 11      | 5'-0"   | 184     | 920'-0"      |        |
| F2   |            | 8       | 9'-6"   | 480     | 4560'-0"     |        |
| F3   |            | 6       | 14'-6"  | 48      | 696'-0"      |        |
| F4   | FOOTINGS   | 4       | 6'-4"   | 72      | 456'-0"      |        |
| H1   | CAPS       | 5       | 12'-6"  | 368     | 4600'-0"     |        |
| H2   |            | 11      | 51'-0"  | 16      | 816'-0"      |        |
| H3   |            | 10      | 51'-0"  | 16      | 816'-0"      |        |
| H4   |            | 11      | 12'-0"  | 24      | 288'-0"      |        |
| H5   |            | 10      | 12'-6"  | 40      | 500'-0"      |        |
| H6   |            | 10      | 8'-3"   | 24      | 198'-0"      |        |
| H7   | CAPS       | 4       | 25'-11" | 32      | 829'-4"      |        |

| MARK | LOCATION  | SIZE NO | LENGTH | NO BARS   | TOTAL LENGTH | SKETCH        |
|------|-----------|---------|--------|-----------|--------------|---------------|
| P1   | PARAPETS  | 4       | 2'-11" | 796       | 2321'-8"     |               |
| P2   |           | 1'-4"   | 828    | 1104'-0"  |              |               |
| P3   |           | 44'-0"  | 48     | 2112'-0"  |              |               |
| P4   |           | 6'-5"   | 16     | 102'-8"   |              |               |
| P5   |           | 4'-6"   | 384    | 1728'-0"  |              |               |
| P5a  |           | 4'-6"   | 8      | 36'-0"    |              |               |
| P5b  |           | 4'-1"   | 8      | 32'-8"    |              |               |
| P5c  |           | 3'-8"   | 8      | 29'-4"    |              |               |
| P5d  |           | 3'-3"   | 8      | 26'-0"    |              |               |
| P6   |           | 2'-6"   | 32     | 80'-0"    |              |               |
| P7   |           | 4       | 4'-5"  | 144       | 636'-0"      |               |
| P8   |           | 6       | 31'-0" | 64        | 1984'-0"     |               |
| P8a  |           | 6       | 26'-8" | 32        | 853'-4"      |               |
| P9   |           | 4       | 2'-8"  | 304       | 810'-8"      |               |
| P10  | PARAPETS  | 4       | 4'-2"  | 8         | 33'-4"       |               |
| S1   | DECK SLAB | 5       | 56'-6" | 370       | 20905'-0"    | SEE SKETCH S1 |
| S2   |           | 54'-10" | 740    | 40576'-8" |              |               |
| S3   |           | 40'-9"  | 640    | 26080'-0" |              |               |
| S4   |           | 26'-3"  | 48     | 1260'-0"  |              |               |
| S5   | DECK SLAB | 5       | 33'-0" | 48        | 1584'-0"     |               |

| MARK | LOCATION  | SIZE NO | LENGTH | NO BARS | TOTAL LENGTH | SKETCH |
|------|-----------|---------|--------|---------|--------------|--------|
| W1   | WING WALL | 4       | 7'-1"  | 24      | 170'-0"      |        |
| W2   |           | 4       | 2'-4"  | 16      | 37'-4"       |        |
| W3   |           | 4       | 2'-10" | 16      | 45'-4"       |        |
| W4   |           | 4       | 3'-6"  | 16      | 56'-0"       |        |
| W5   |           | 4       | 4'-2"  | 16      | 66'-8"       |        |
| W6   |           | 4       | 4'-11" | 16      | 78'-8"       |        |
| W7   |           | 4       | 5'-7"  | 16      | 89'-4"       |        |
| W8   |           | 4       | 6'-3"  | 16      | 100'-0"      |        |
| W9   |           | 4       | 6'-11" | 16      | 110'-8"      |        |
| W10  |           | 5       | 9'-10" | 32      | 314'-8"      |        |
| W11  |           | 8'-9"   | 16     | 140'-0" |              |        |
| W12  |           | 7'-3"   | 16     | 116'-0" |              |        |
| W13  |           | 5'-9"   | 16     | 92'-0"  |              |        |
| W14  |           | 4'-1"   | 16     | 65'-4"  |              |        |
| W15  | WING WALL | 5       | 2'-8"  | 16      | 42'-8"       |        |
| W16  | WING WALL | 5       | 9'-0"  | 16      | 144'-0"      |        |

65,512'-2" OF #4 BARS @ 0.668 #/FT= 43,762 LB  
96,883'-0" OF #5 BARS @ 1.043 #/FT= 101,048 LB  
5,357'-4" OF #6 BARS @ 1.502 #/FT= 8,046.7 LB  
4,817'-4" OF #7 BARS @ 2.044 #/FT= 9,846.6 LB  
8,418'-0" OF #8 BARS @ 2.670 #/FT= 22,476.1 LB  
1,514'-0" OF #10 BARS @ 4.303 #/FT= 6,514.7 LB  
30,079'-4" OF #11 BARS @ 5.313 #/FT= 159,811.5 LB  
TOTAL= 351,506.6 LB

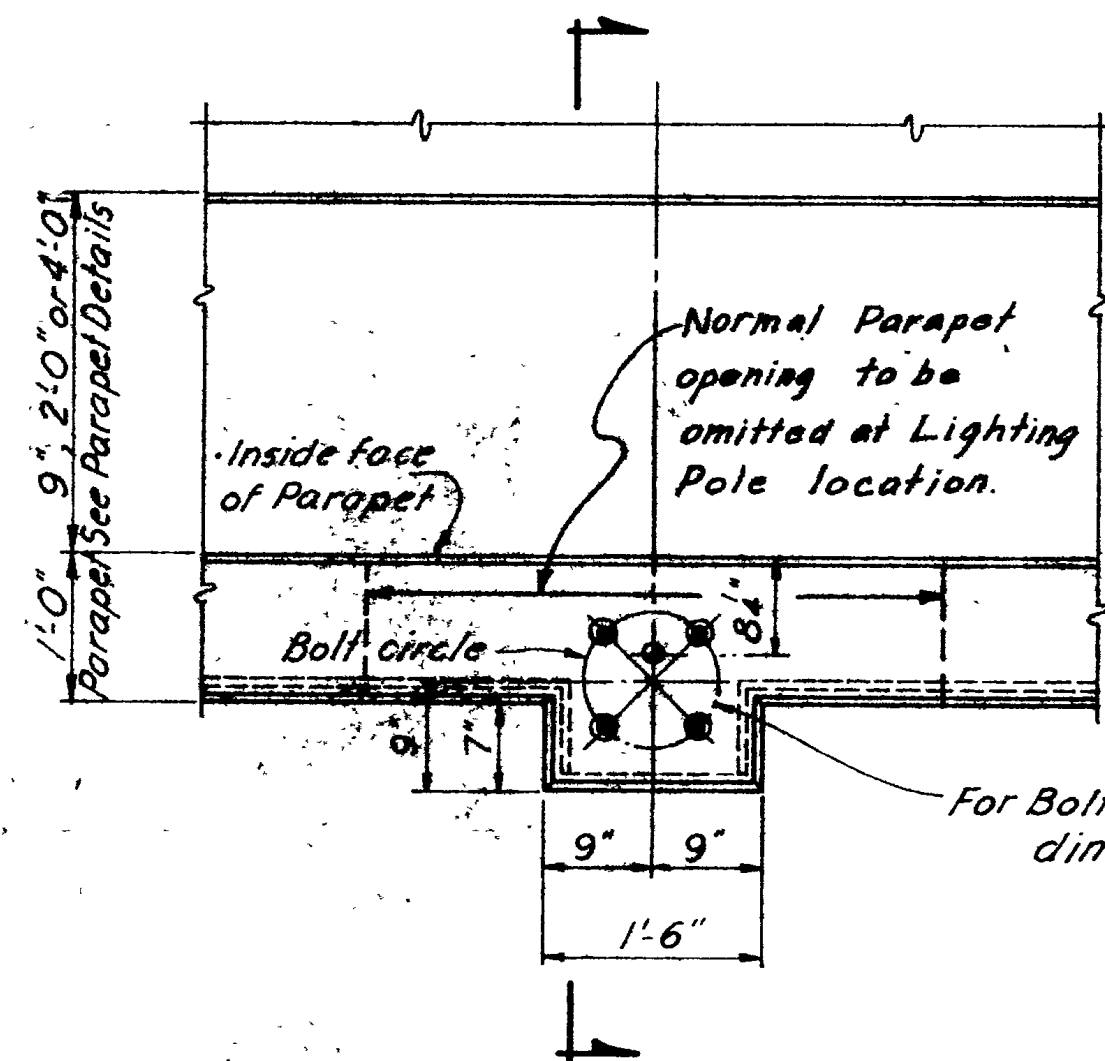


UTAH STATE DEPARTMENT OF HIGHWAYS  
SALT LAKE CITY, UTAH  
STRUCTURES DIVISION  
HILLFIELD INTERCHANGE TO DAVIS-WEBER CO. LINE  
HILL FIELD RD INTERCH. O'PASS  
REINFORCING STEEL SCHEDULE

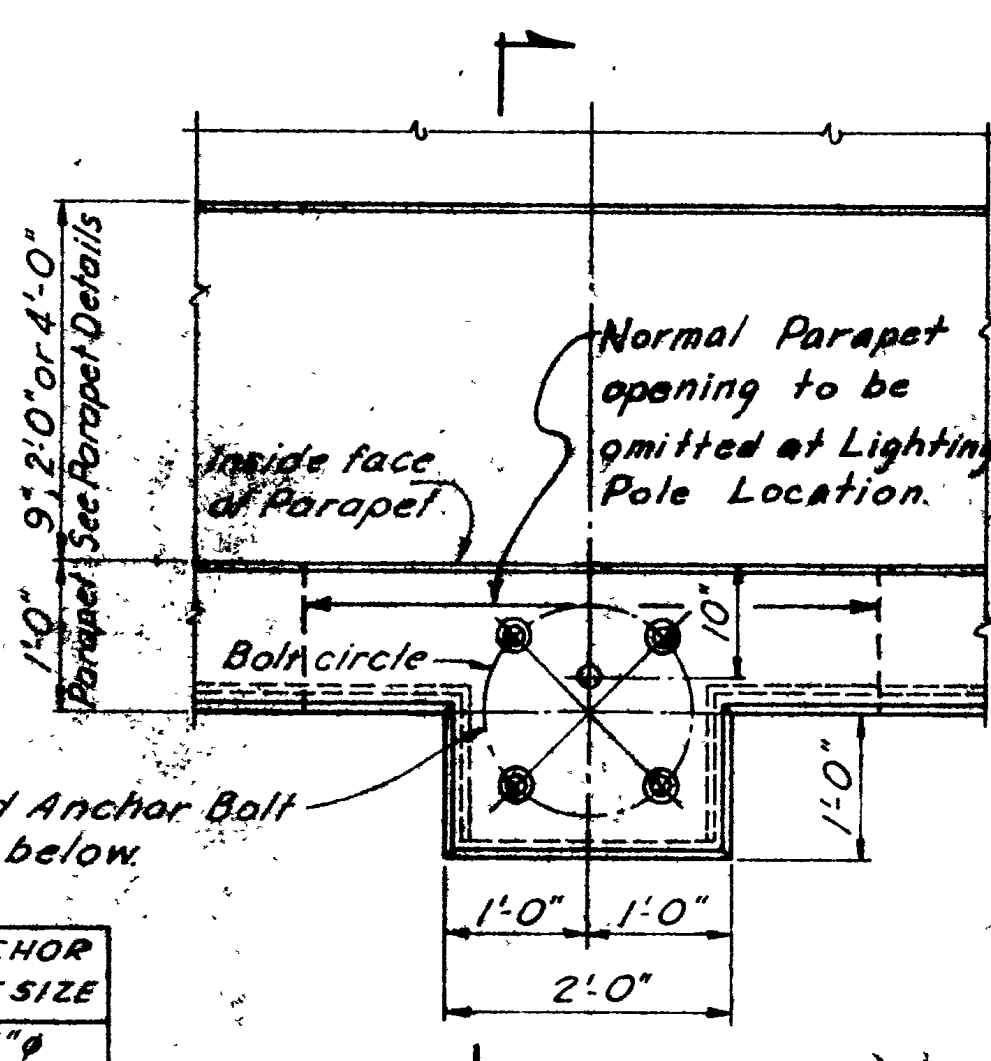
DESIGNED BY: M.W. CHECKED BY: M.J.C. PROJECT NUMBER: 1-15-7(58)332  
DRAWN BY: U.M.S. CHECKED BY: B.M.W. QUANTITIES BY: H.J.S. CHECKED BY: B.M.W. STATION: 729 + 62.72  
APPROVAL: [Signature] DATE: [Date] COUNTY: DAVIS  
APPROVED: [Signature] DATE: [Date] COUNTY: DAVIS

NO. BY DATE REVISIONS

BR NO. ORG NO. D-719 12 OF 13



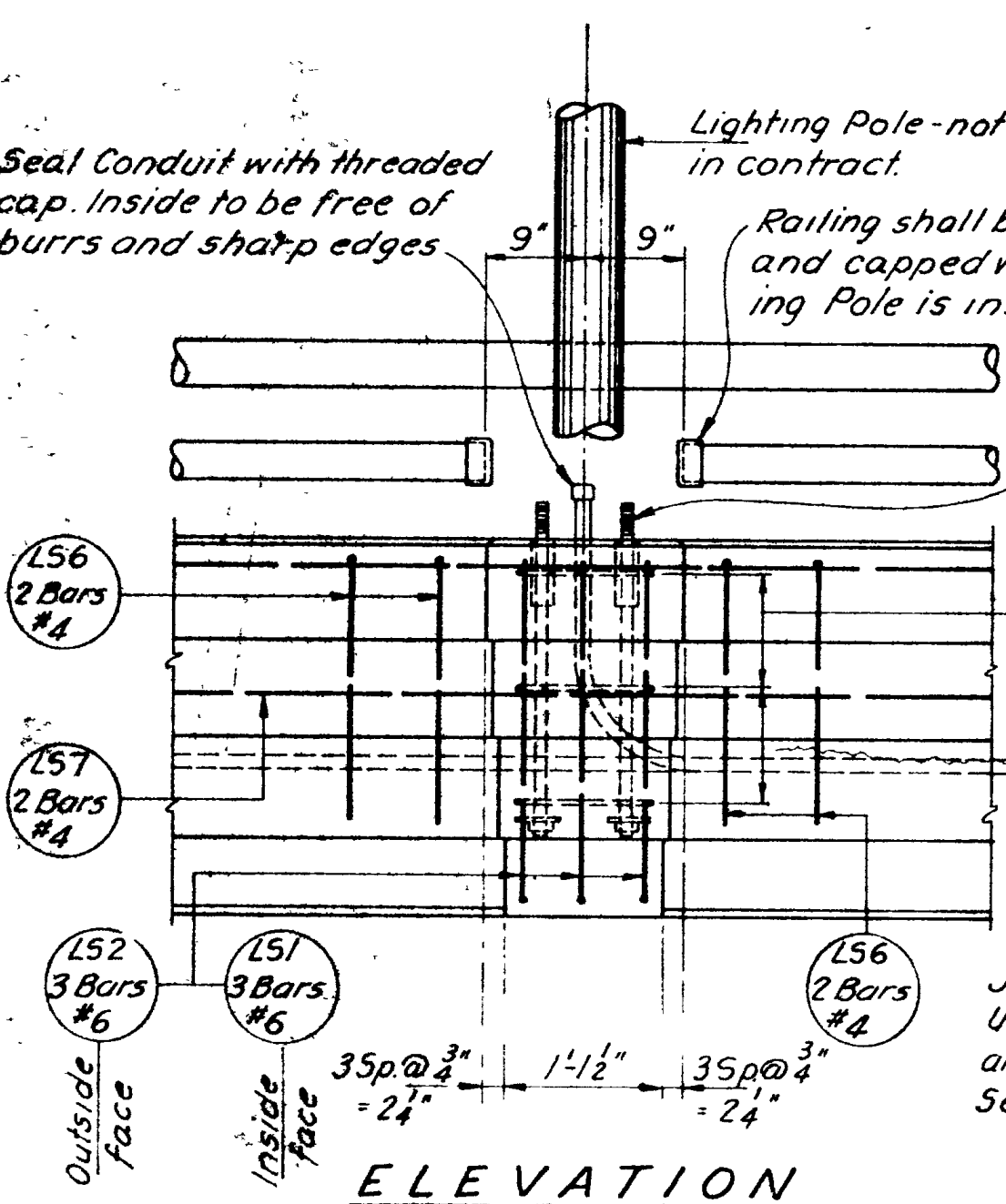
PLAN (BRACKET "A")



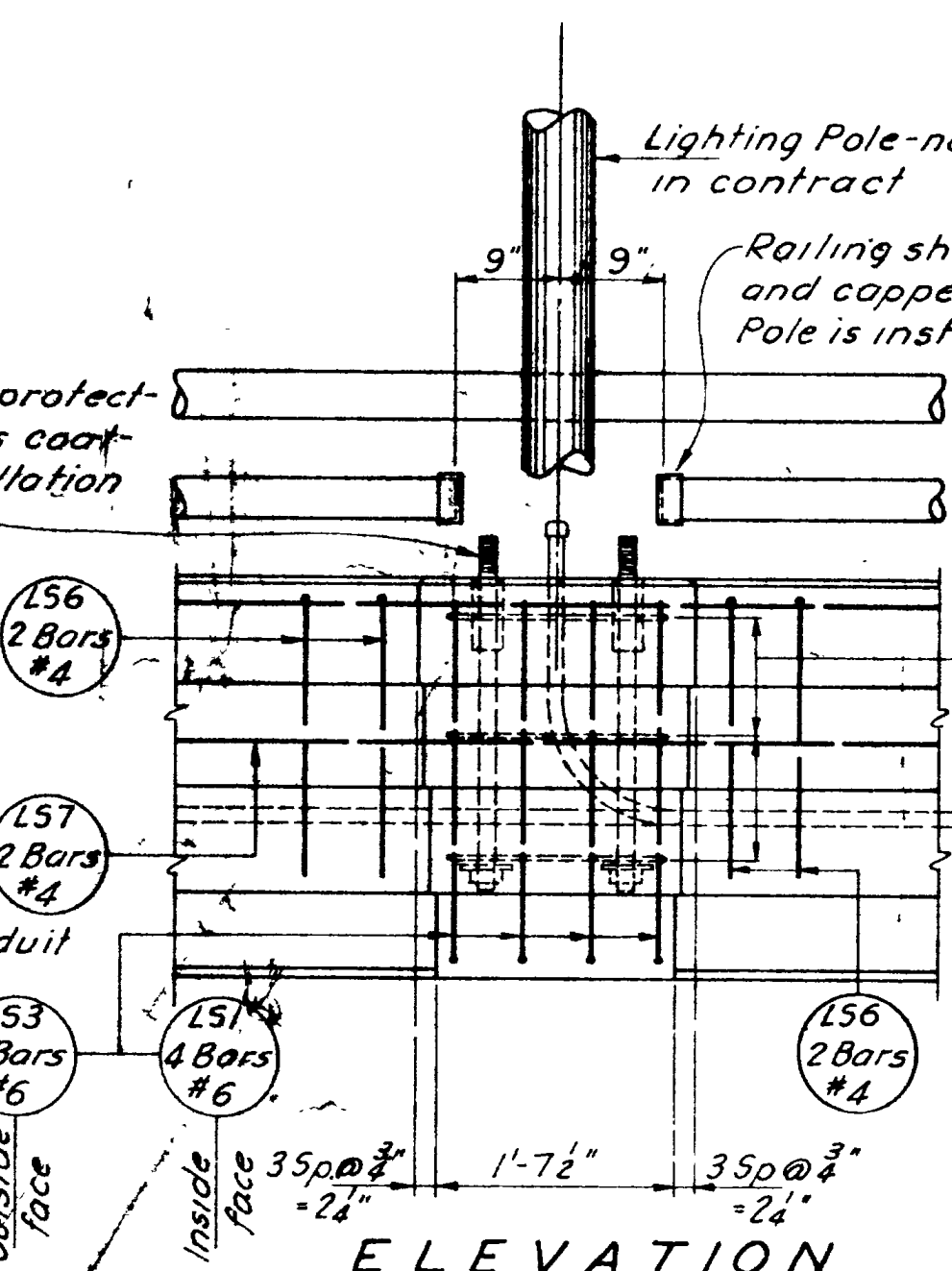
PLAN (BRACKET "B")

| BOLT CIRCLE DIA. | ANCHOR BOLT SIZE |
|------------------|------------------|
| 11"              | 1"φ              |
| 15"              | 1 1/2"φ          |
| 17 1/2"          | 1 3/4"φ          |

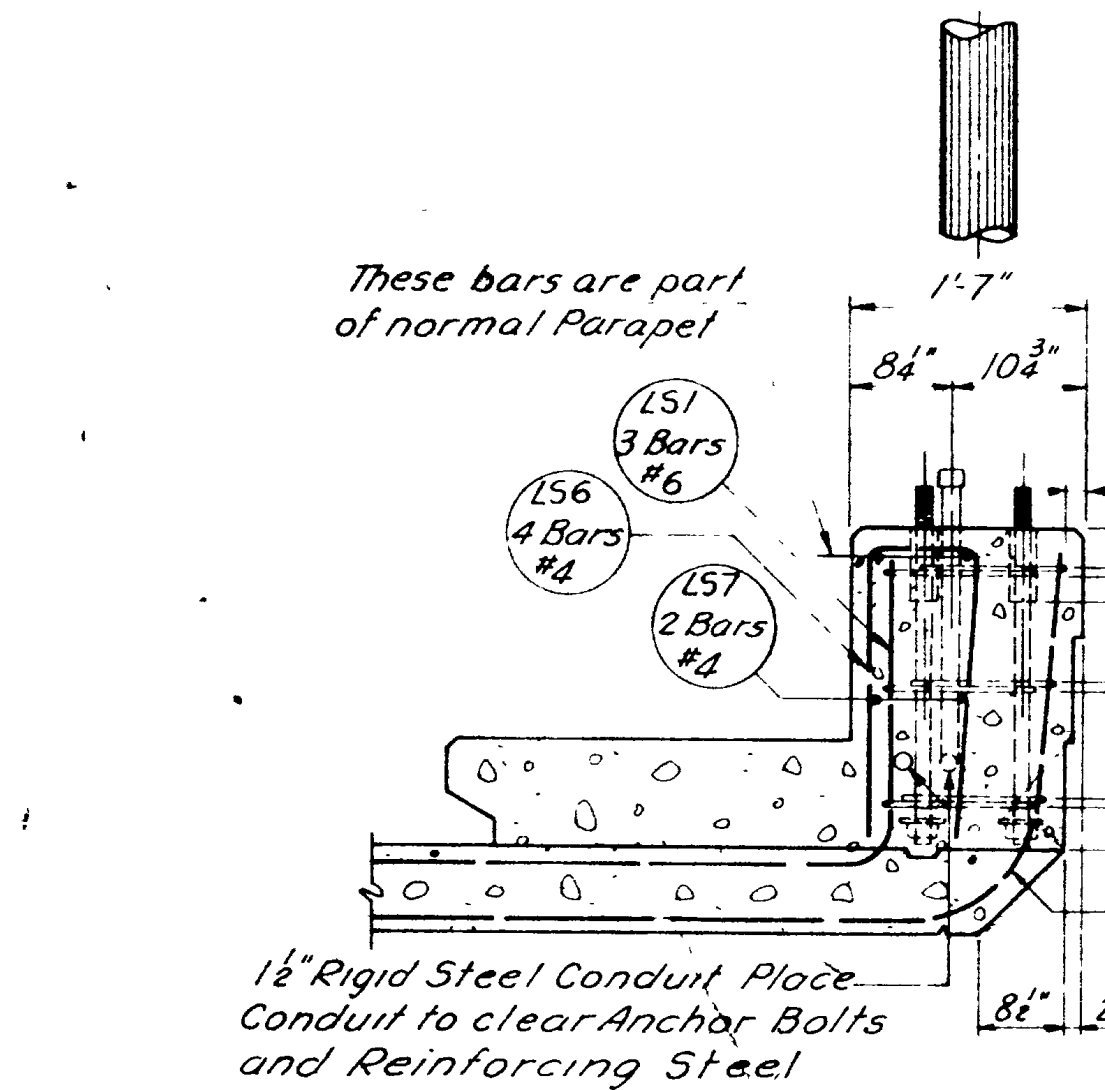
For Bolt Circle diameter and Anchor Bolt dimensions see Table below.



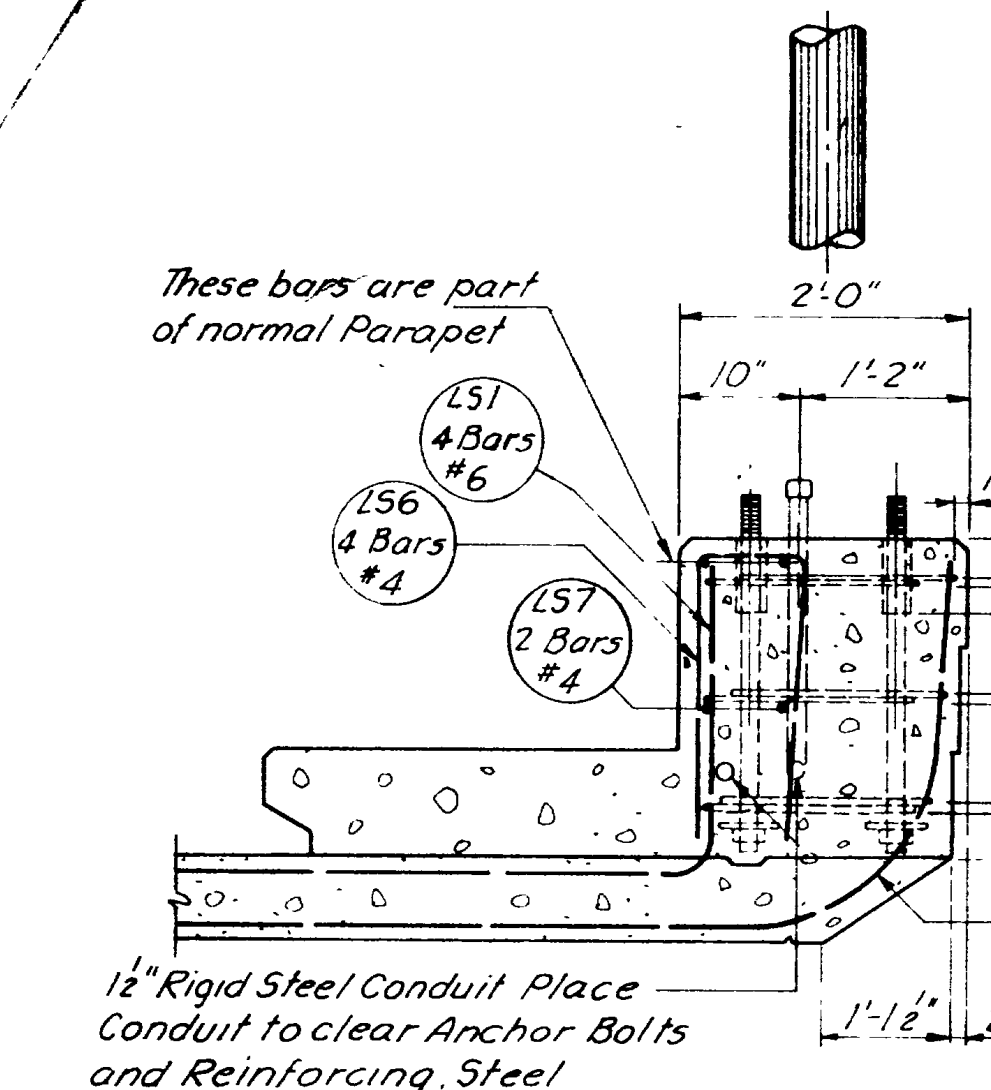
ELEVATION



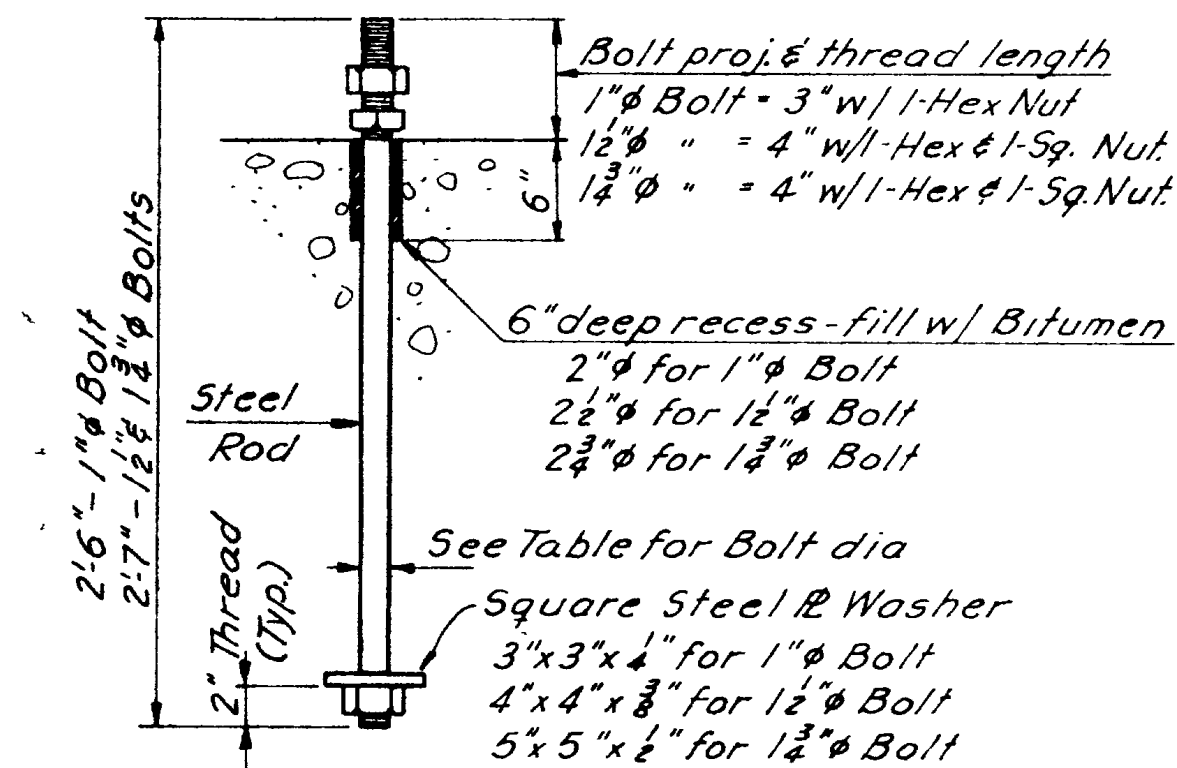
ELEVATION



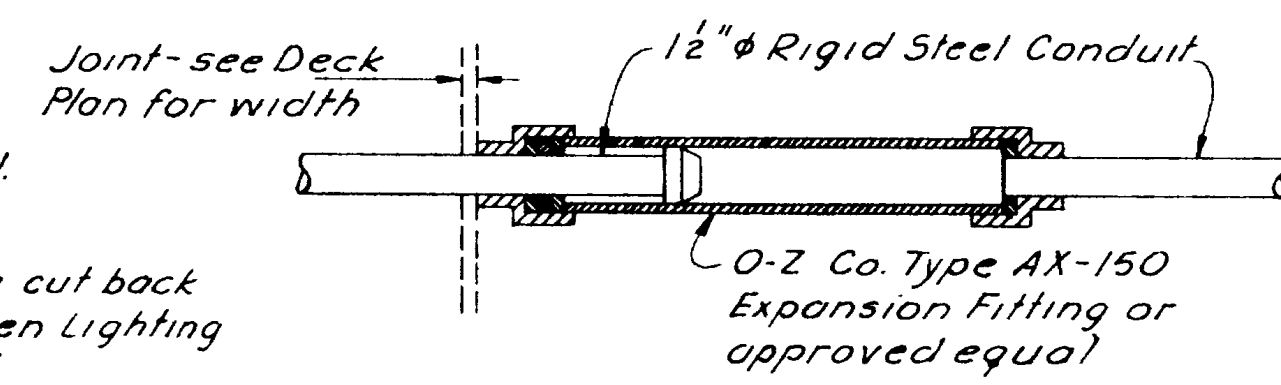
SECTION A-A



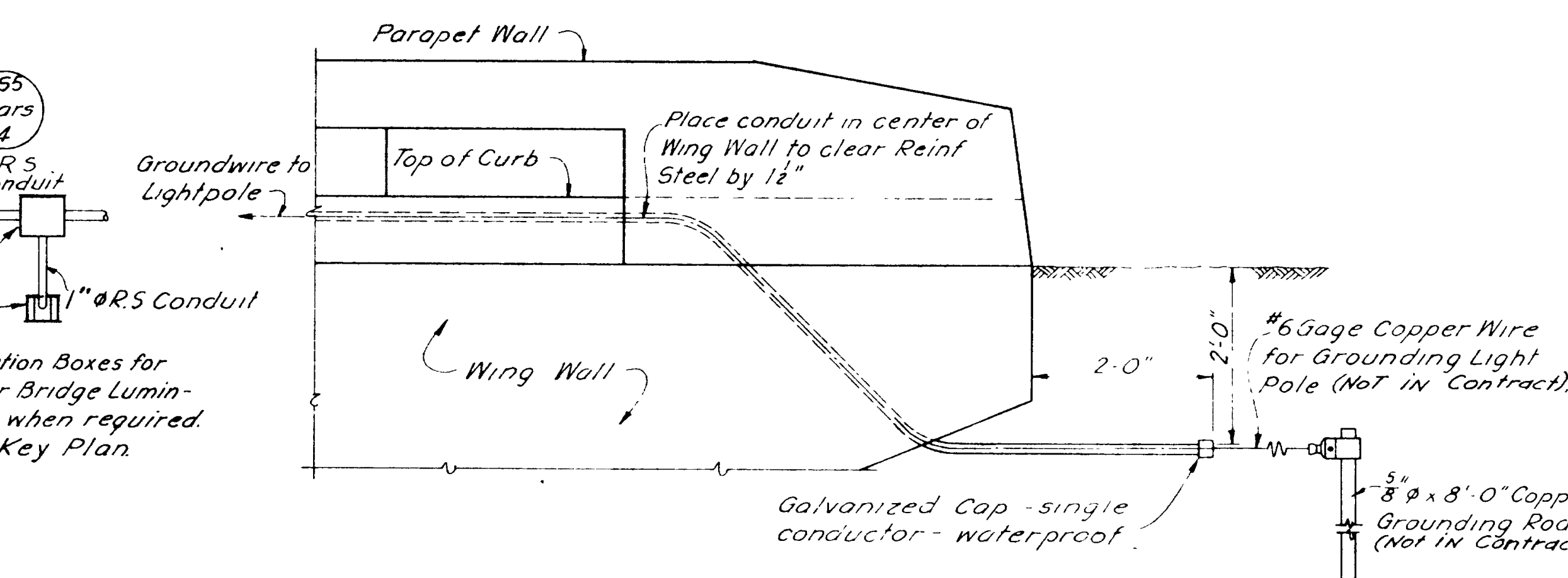
SECTION B-B



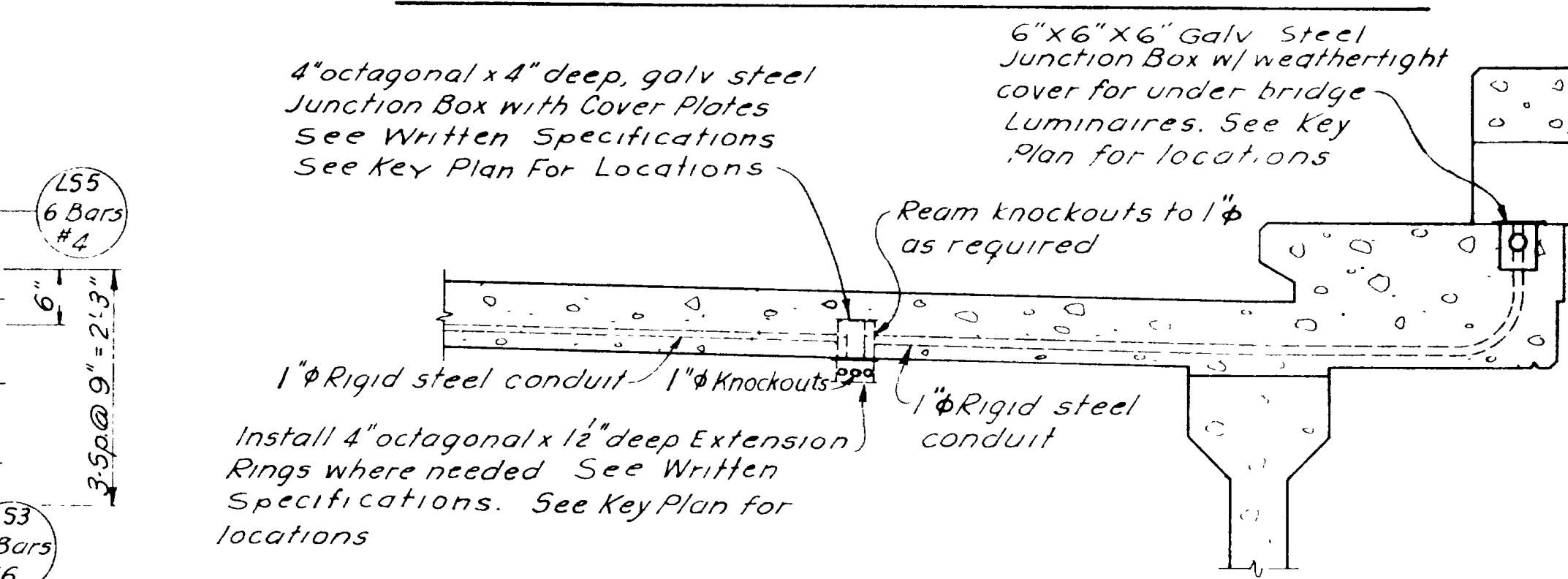
ANCHOR BOLT DETAIL



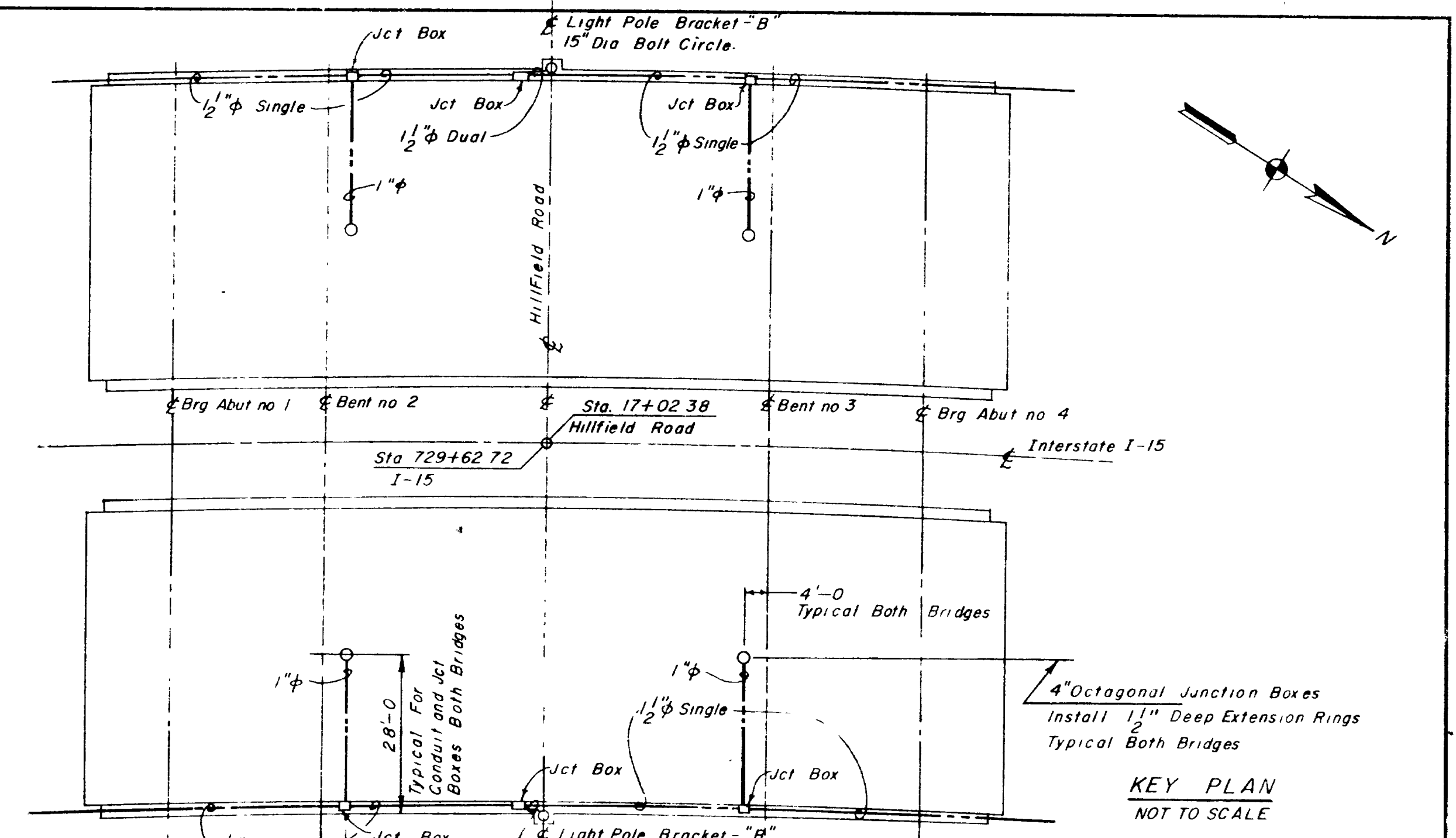
EXPANSION FITTING DETAIL



CONDUIT TERMINATION AT WING WALLS



DETAILS FOR FUTURE SUSPENDED UNDERPASS LUMINAIRES



KEY PLAN NOT TO SCALE

| STEEL SCHEDULE - POLE SUPPORT |      |        |     |        |
|-------------------------------|------|--------|-----|--------|
| MARK                          | SIZE | LENGTH | AMT | SKETCH |
| L51                           | 6    | 5'-2"  | 8   |        |
| L52                           | 6    | 6'-1"  |     |        |
| L53                           | 6    | 6'-4"  | 8   |        |
| L54                           | 4    | 3'-3"  |     |        |
| L55                           | 4    | 4'-8"  | 12  |        |
| L56                           | 4    | 4'-11" | 8   |        |
| L57                           | 4    | 8'-0"  | 4   |        |

GENERAL NOTES

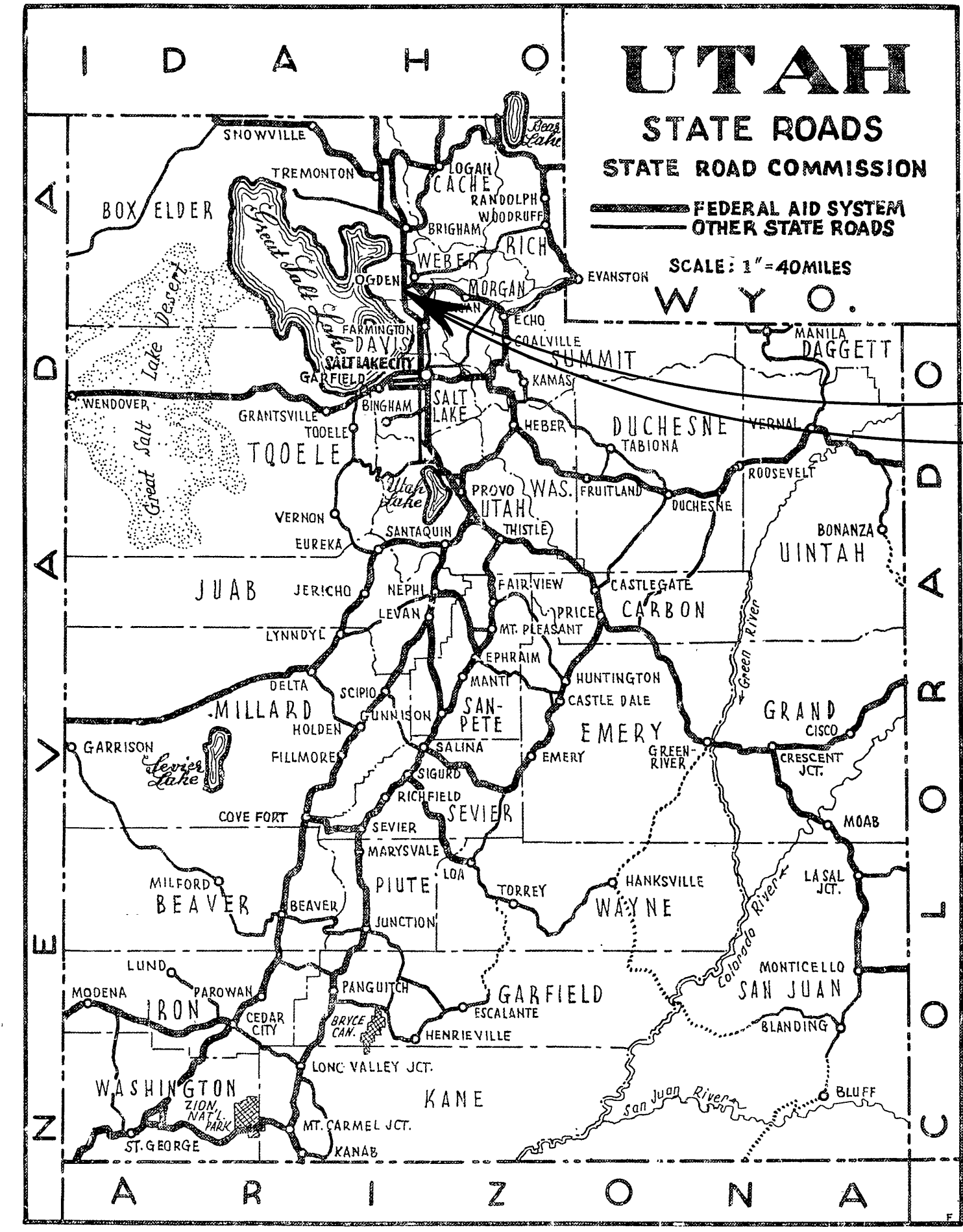
- 1-Rigid Steel Conduit, threaded and galvanized, shall conform to Federal Specifications WW-C-581c
- 2-Conduit Expansion Fittings shall be installed where ever conduit crosses structure expansion joints
- 3-Conduit shall terminate at junction boxes with double lock nuts and insulated grounding type bushings. Bushings shall be O-Z Co Type BL or approved equal. All conduit shall be bonded in boxes with bare #6 AWG Copper wire
- 4-All work shall conform to the latest Edition of the National Electrical Code
- 5-See Special Provisions for Electrical Work - Bridges

| UTAH STATE DEPARTMENT OF HIGHWAYS                                       |                      |                |  |
|---|----------------------|----------------|--|
| SALT LAKE CITY, UTAH  |                      |                |  |
| STRUCTURES DIVISION   |                      |                |  |
| HILLFIELD INTERCHANGE TO DAVIS-WEBER CO. HILLFIELD RD INTERCH. 30' PASS |                      |                |  |
| LIGHTING DETAILS FOR STRUCTURE  |                      |                |  |
| DESIGNED BY   | CHECKED BY           | 1-15-75(8)332  |  |
| DRAWN BY J.H.O. VW  | CHECKED BY           | PROJECT NUMBER |  |
| QUANTITIES BY   | CHECKED BY           | 729+62.72      |  |
| APPROVAL RECOMMENDED BY Bent I Taylor                                   | GROUP LEADER         | STATION        |  |
| APPROVED DATE   | CHIEF STRUCTURAL ENG | DAVIS COUNTY   |  |
| BR NO   | DRG NO D-719         | 13 OF 13       |  |



| FED. ROAD DIST. NO. | STATE | FED. AID PROJ. NO.               | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|----------------------------------|-------------|-----------|--------------|
| UTAH                | UTAH  | 1-15-7(58) 332<br>1-15-8(20) 334 | 1963        | 1         |              |

# STATE OF UTAH STATE ROAD COMMISSION



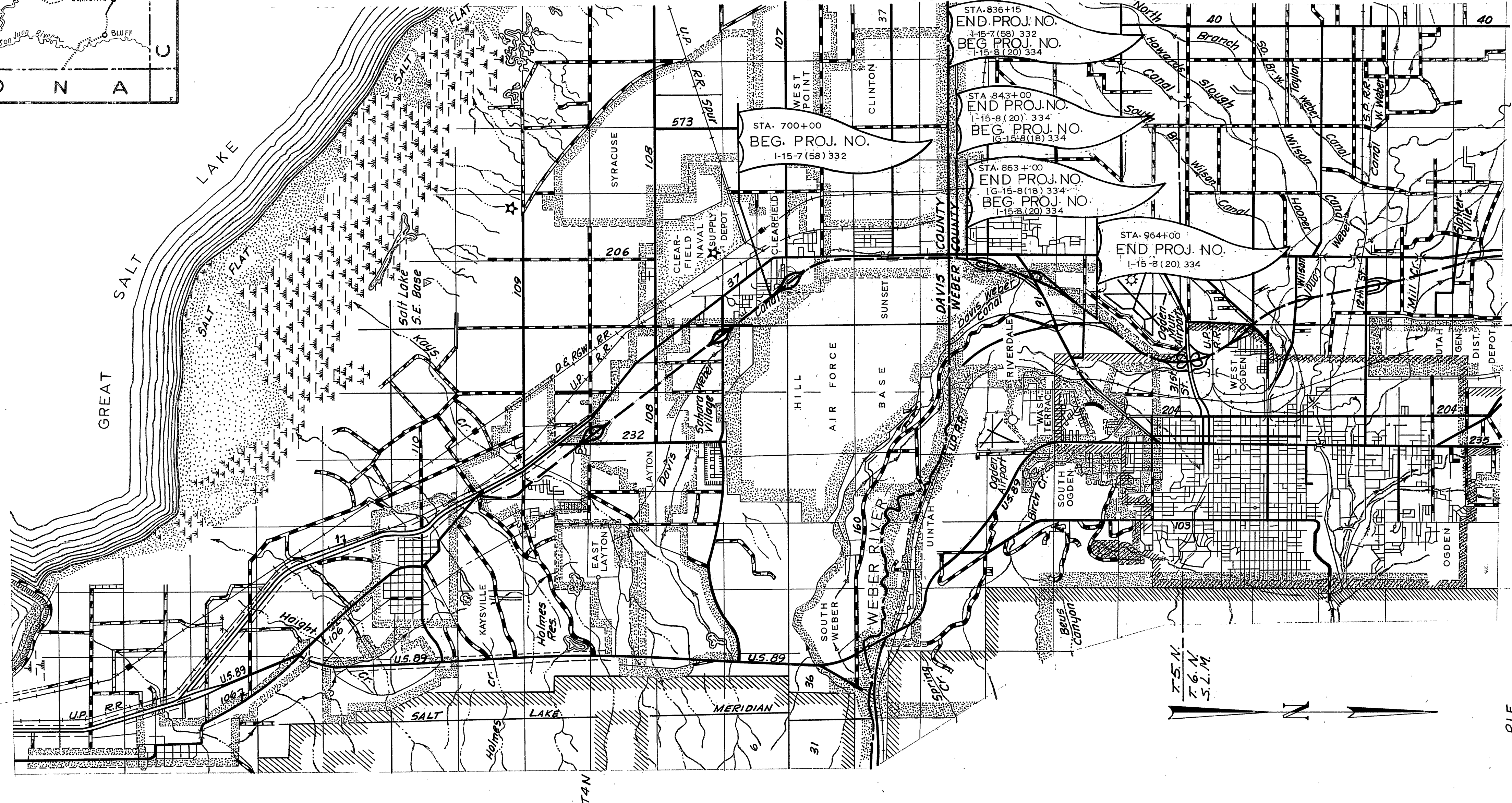
I-15-7(58) 332  
I-15-8(20) 334  
IG-15-8(18) 334

## PLANS OF PROPOSED STATE ROAD

**FEDERAL AID PROJECT**  
 I-15-7 (58) 332  
 LENGTH = 2.585 MILES  
 DAVIS COUNTY  
 I-15-8 (20) 334  
 LENGTH = 2.043 MILES  
 WEBER COUNTY  
 IG-15-8(18) 334  
 LENGTH = 0.380 MILES  
 WEBER COUNTY

INDEX TO SHEETS

| SHEET NO. | DESCRIPTION                    | DRAWING NO. | STATION    |
|-----------|--------------------------------|-------------|------------|
| 1         | TITLE SHEET                    |             |            |
| 2A-C      | TYPICAL SECTIONS & MAT'L S. SH |             |            |
| 3A-E      | SUMMARY SHEETS                 |             |            |
| 4-31      | PLAN AND PROFILE               |             |            |
| 32-44     | HILL FIELD OVERPASS            | D-719       | 729+62.75  |
| 45-69     | UPRR. OVERPASS                 | F-80        | 854+70     |
| 70-95     | 5600 SOUTH OVERPASS            | F-66        | 859+81.30  |
| 96-101    | 5600 SOUTH CANAL X-ING         | D-744       | 1270+92.00 |
| 102-131   | RIVERDALE ROAD UNDERPASS       | C-453       | 904+43.80  |
| 132-145   | 4400 SOUTH UNDERPASS           | D-735       | 955+28.84  |
| 146       | STD. RAILING DETAILS           | MR-1        |            |
| 147       | CONCRETE PILES                 | SP-1        |            |
| 148       | STD. STEEL DETAILS             | SS-3        |            |
| 149       | STD. CATCH BASINS              | V-752       |            |
| 150       | STD. CATCH BASINS              | V-753       |            |
| 151       | STD. CLEANOUT BOXES            | V-777       |            |
| 152-156   | STD. ROAD SIGNS                | 500-1-5     |            |
| 157       | STD. FLARED END SEC. RCP.      | B-160-1     |            |
| 158       | RIGHT OF WAY MARKERS           | B-164       |            |
| 159       | SUPERELEVATION & WIDENING      | 202         |            |
| 160       | LEFT TURN MEDIANS              | 211         |            |
| 161       | CONCRETE CURB & GUTTER         | 220         |            |
| 162       | DEEP BEAM GUARD RAIL           | 230         |            |
| 163       | RCP CULVERTS                   | 320         |            |
| 164       | DROP INLETS                    | 346         |            |
| 165       | CONSTRUCTION IDENT. SIGNS      | 504         |            |
| 166       | CHAIN LINK FENCE               | 523         |            |



UTAH STATE DEPARTMENT OF HIGHWAYS  
 RECOMMENDED FOR APPROVAL: AUG., 1963  
*Blaine J. Kay*  
 ENGINEER OF ROADWAY DESIGN  
 RECOMMENDED FOR APPROVAL: AUG., 1963  
*Blaine J. Kay*  
 ENGINEER OF PLANS AND CONTRACTS  
 APPROVED: AUG., 1963  
*ACT. DIRECTOR OF HIGHWAYS*

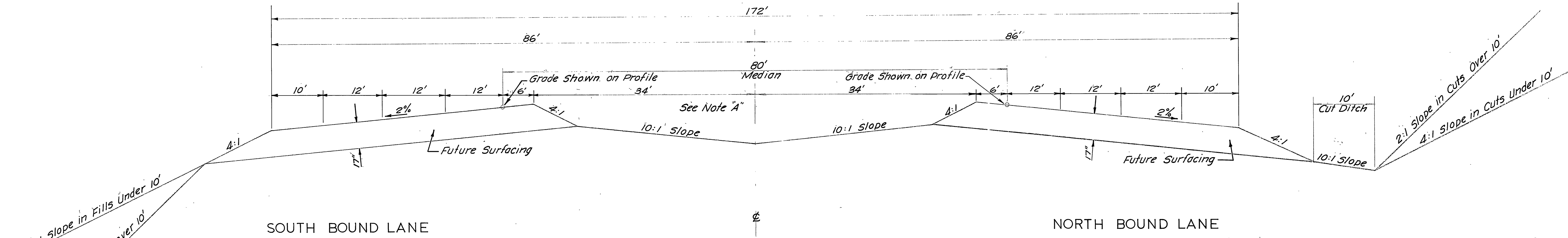
DEPARTMENT OF COMMERCE  
 BUREAU OF PUBLIC ROADS  
 APPROVED: \_\_\_\_\_  
 DIVISION ENGINEER DATE



# TYPICAL SECTIONS

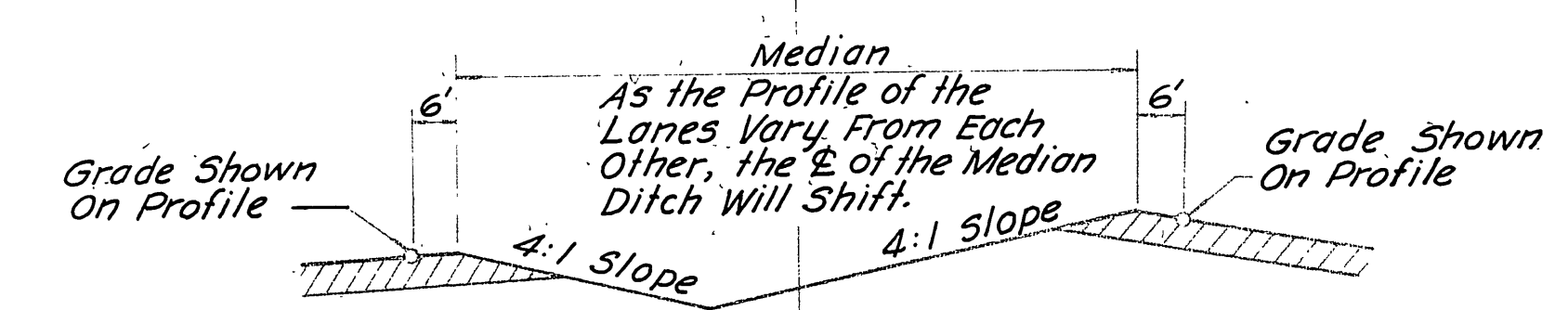
| STATE          | DIST. NO. | SECTION   | SHEET NO. | TOTAL SHEETS |
|----------------|-----------|-----------|-----------|--------------|
| UTAH           | 15        | 7(68) 332 | 2         | 2            |
| UTAH           | 15        | 8(20) 334 |           |              |
| 16-15-8(18)334 |           |           |           |              |

### NO. 1



STA. 700+00 TO STA. 829 + 79.30 S.B. LANE  
 STA. 700+00 TO STA. 830 + 86.27 NB LANE

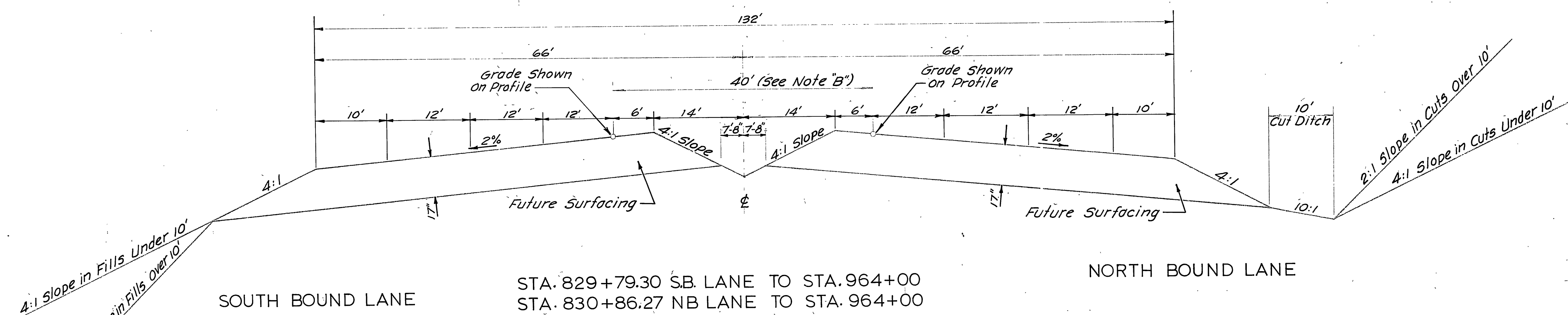
NORTH BOUND LANE



NOTE "A"  
 STA. 700+00 TO STA. 964+00

NOTE "B"  
 Median Width Varies From 80' to 40' in Transition Through Curve. See Sheet Number 13 For Details.

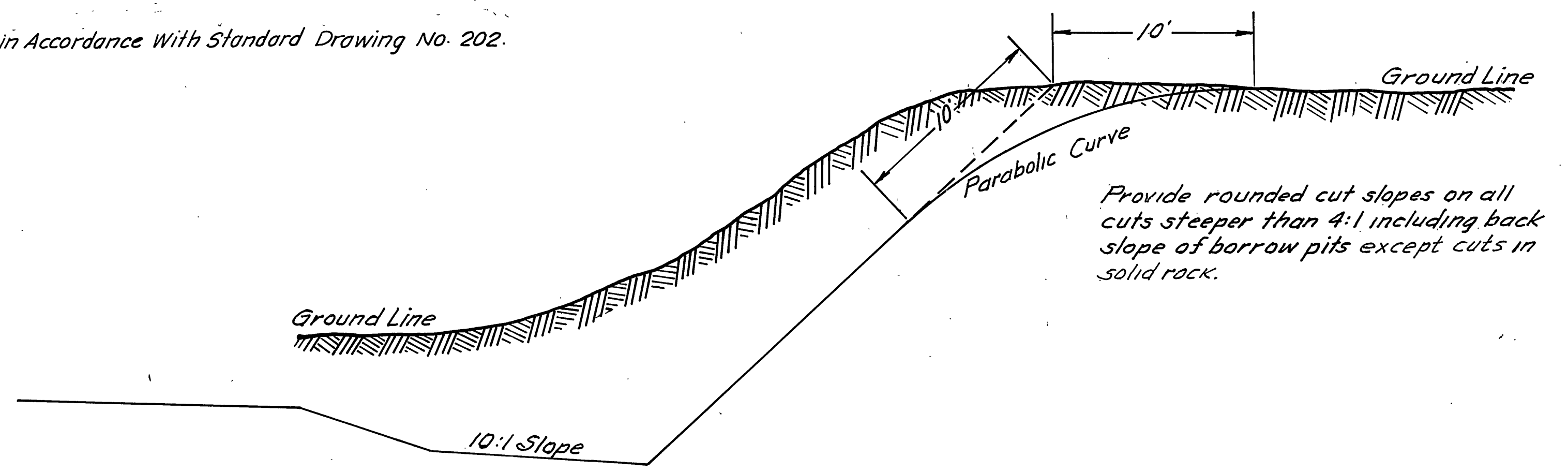
### NO. 2



STA. 829+79.30 S.B. LANE TO STA. 964+00  
 STA. 830+86.27 NB LANE TO STA. 964+00

Design Speed 70 MPH. Superelevate in Accordance With Standard Drawing No. 202.

NOTE  
 SURFACING OF INTERSTATE NOT INCLUDED IN THIS CONTRACT.

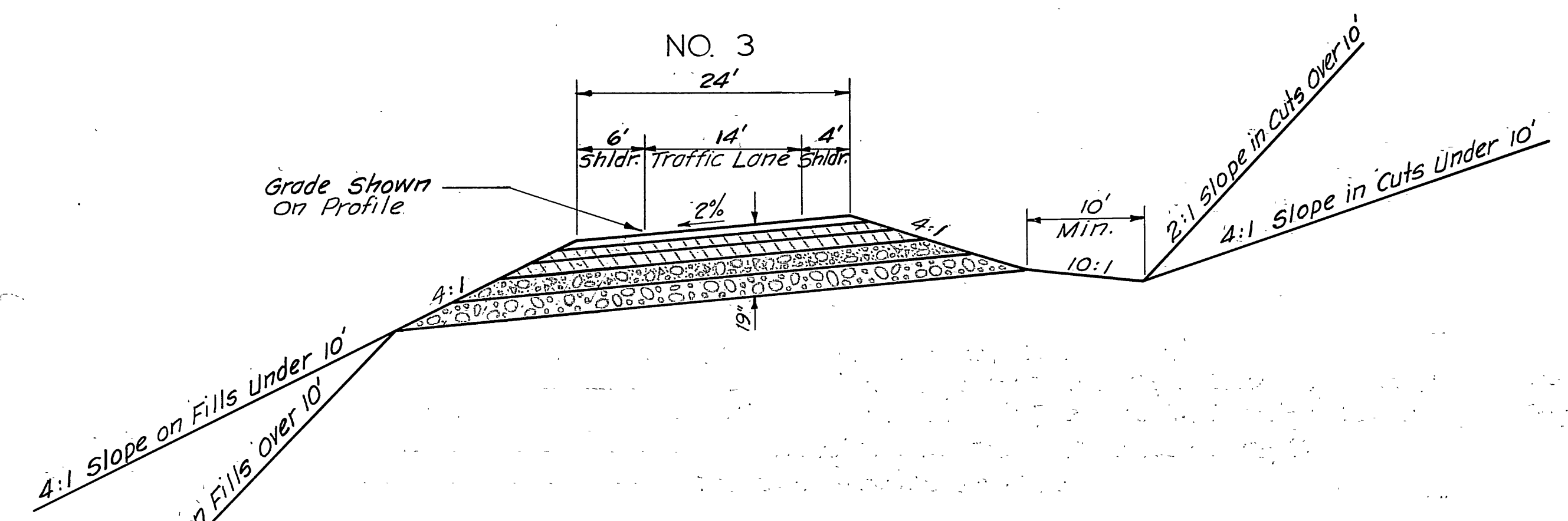




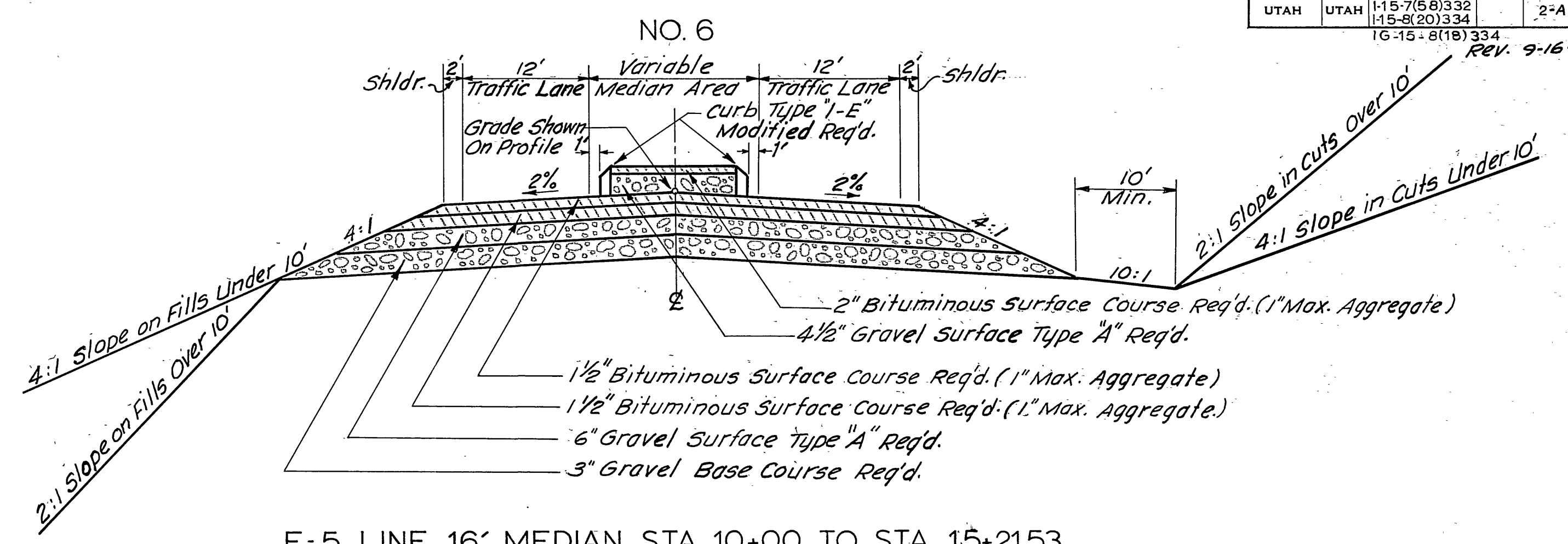
# TYPICAL SECTIONS

| FED. ROAD DIST. NO. | STATE | PROJ. NO.  | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|--|-------------|-----------|--------------|
| UTAH                | UTAH  | 1-15-7(58)332<br>1-15-9(20)334<br>16-15-8(18)334 |             | 2-A       |              |

REV. 9-16-63

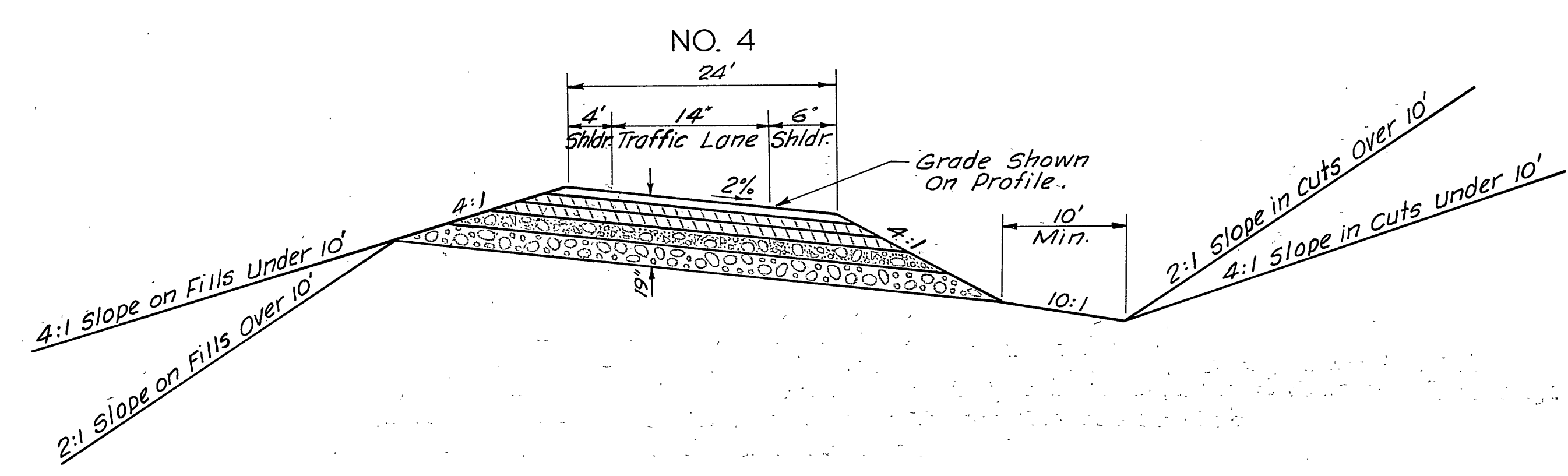


NO. 3  
 F-1 LINE STA. 10+00 TO STA. 19+51.34 - GRADE AND DRAIN ONLY  
 F-2 LINE STA. 10+00 TO STA. 20+75.13 - GRADE AND DRAIN ONLY  
 F-4 LINE STA. 10+00 TO STA. 14+42.16 - GRADE AND DRAIN ONLY  
 T LINE STA. 10+01.03 TO STA. 41+30.02 - GRADE AND DRAIN ONLY

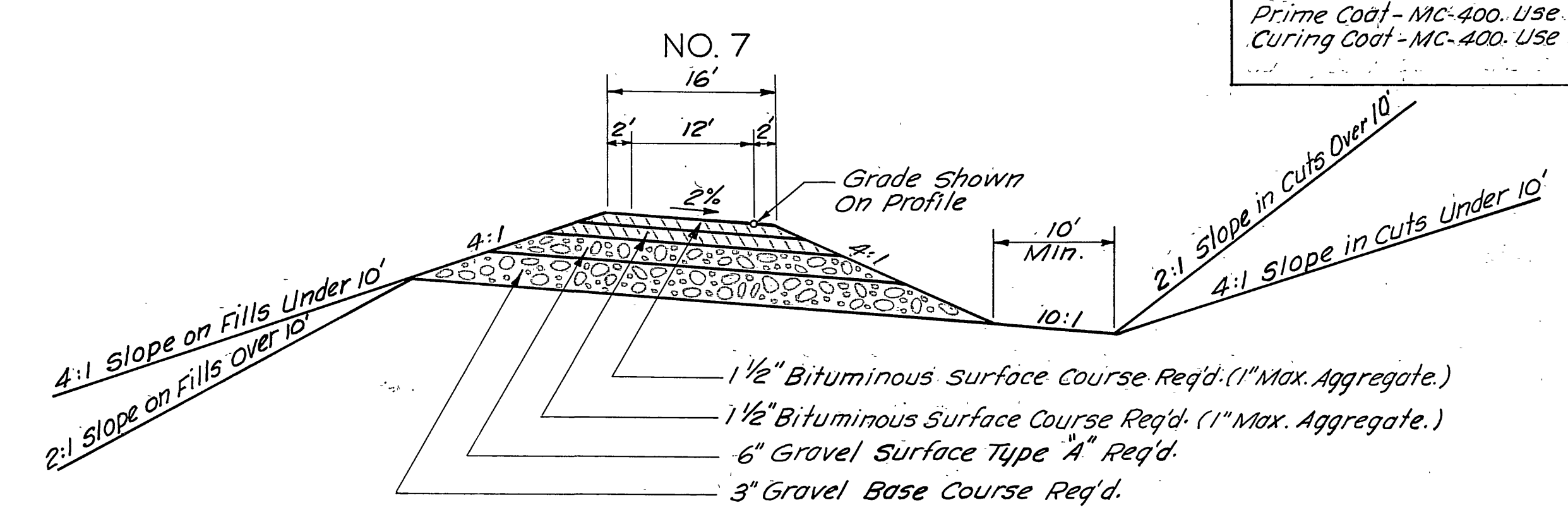


NO. 6  
 F-5 LINE 16' MEDIAN STA. 10+00 TO STA. 15+21.53  
 F-5 LINE VAR. MEDIAN STA. 15+21.53 TO STA. 20+54.78  
 F-5 LINE ZERO MEDIAN STA. 20+54.78 TO STA. 57+08.35

**BITUMINOUS MATERIAL**  
 Plant Mix - Penetration 60-70. Use 5.5% Gravel By Weight.  
 Seal Coat - RC-800. Use .25 Gal./Sq. Yd. With Type A Cover Material.  
 Tack Coat - RC-70. Use .15 Gal./Sq. Yd. Between Plant Mix Bituminous Courses.  
 Prime Coat - MC-400. Use 0.5 Gal./Sq. Yd.  
 Curing Coat - MC-400. Use 0.15 Gal./Sq. Yd.

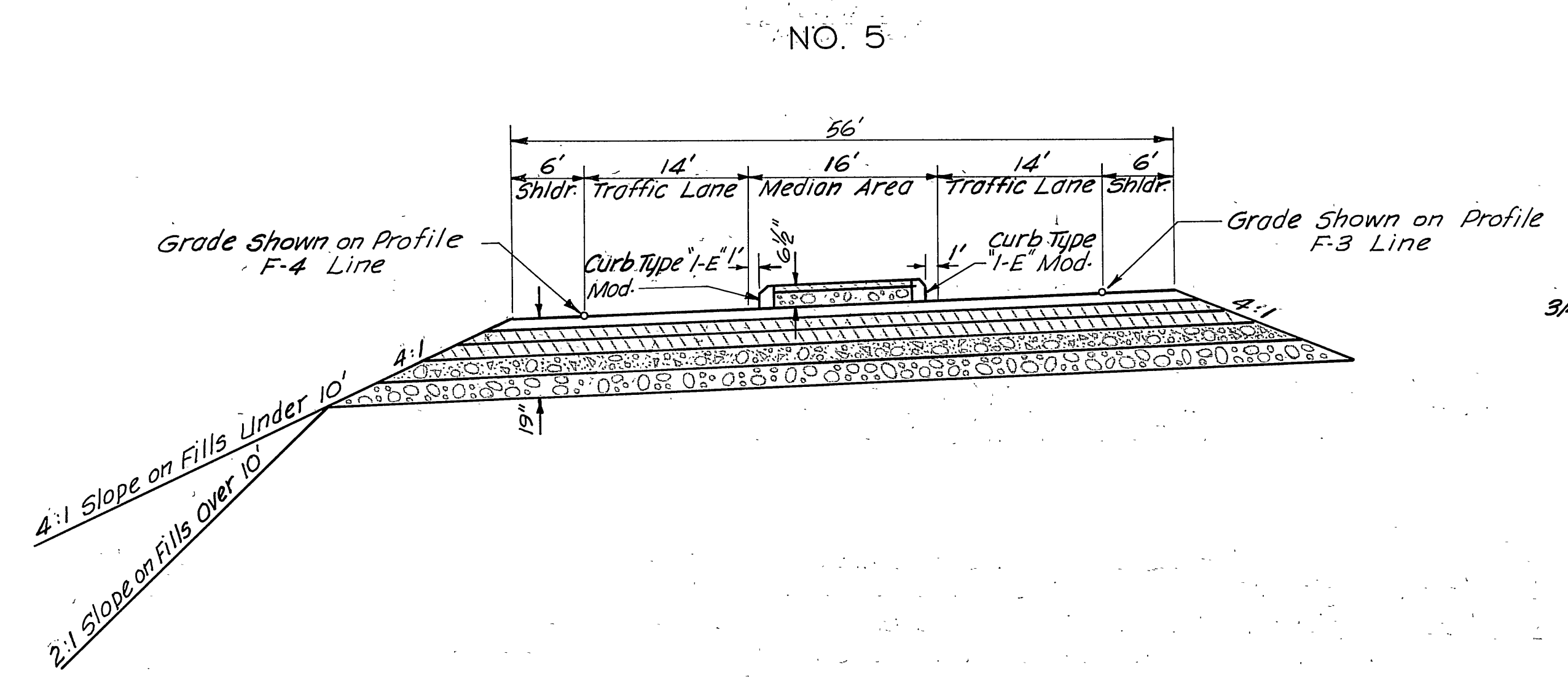


NO. 4  
 F-3 LINE STA. 10+00 TO STA. 19+00 — GRADE AND DRAIN ONLY  
 U LINE STA. 10+00 TO STA. 41+67.29 — GRADE AND DRAIN ONLY  
 R-1 LINE STA. 10+00 TO STA. 23+28.66 - GRADE AND DRAIN ONLY  
 R-2 LINE STA. 10+00 TO STA. 17+90.07 - GRADE AND DRAIN ONLY

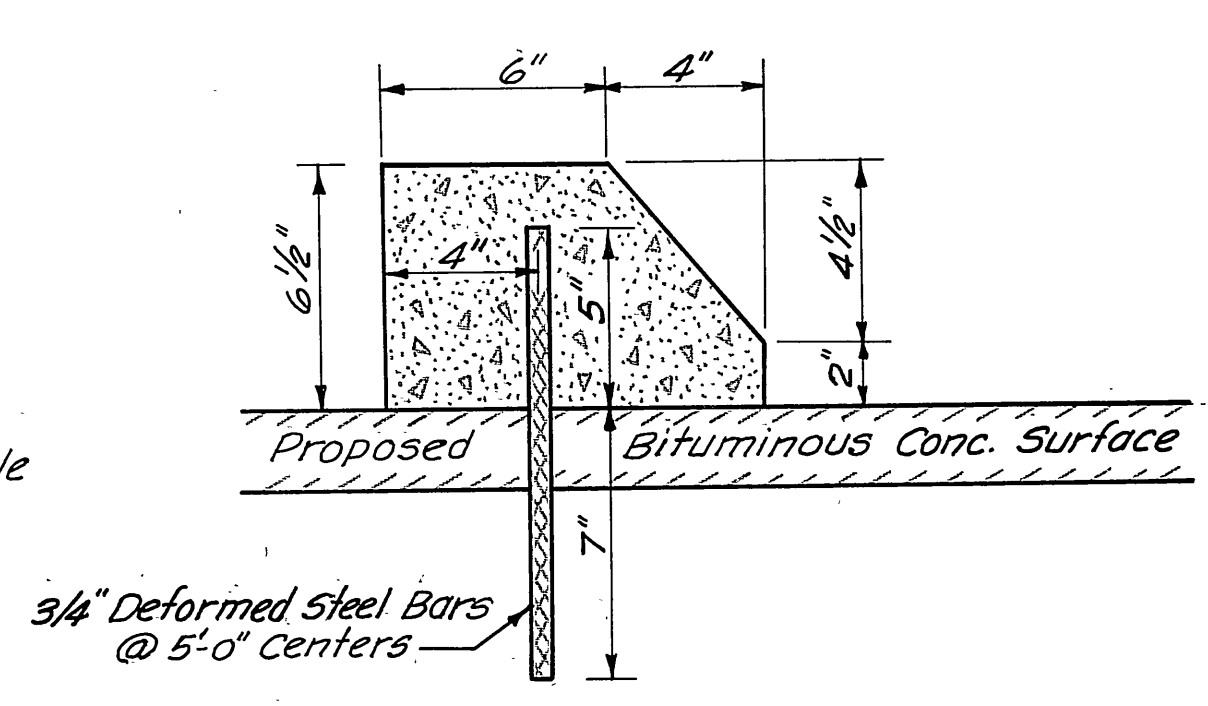


NO. 7  
 F-5A LINE STA. 10+00 TO STA. 12+87

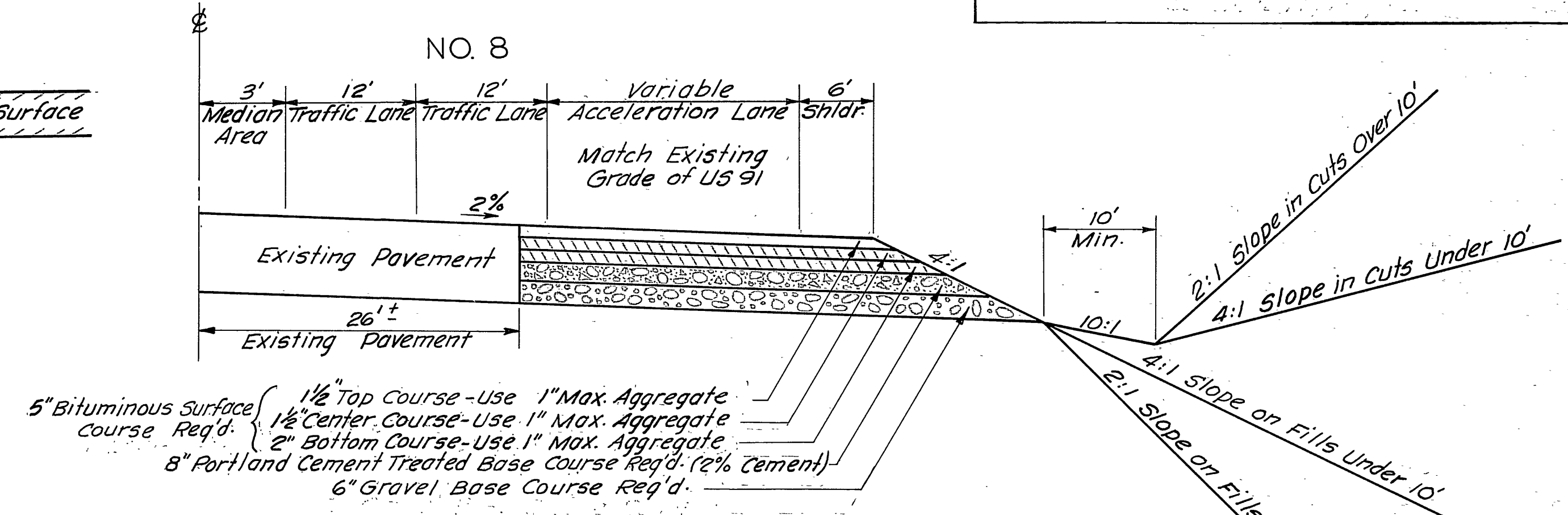
**GRAVEL MATERIAL**  
 Cover Material - Type A - 25 #/sq. yd.  
 Gravel Surface - Type A - 140 #/cu. ft. compacted.  
 Portland Cement (2% by weight) Treated Base Course - 140 #/cu. ft. compacted.  
 Gravel Base Course - 140 #/cu. ft. compacted.  
 Bituminous Surface Course - 148 #/cu. ft.



NO. 5  
 F-3 LINE STA. 19+00 TO STA. 25+10.15 - GRADE AND DRAIN ONLY  
 F-4 LINE STA. 14+42.16 TO STA. 20+57.88 - GRADE AND DRAIN ONLY



CURB TYPE 1-E MODIFIED

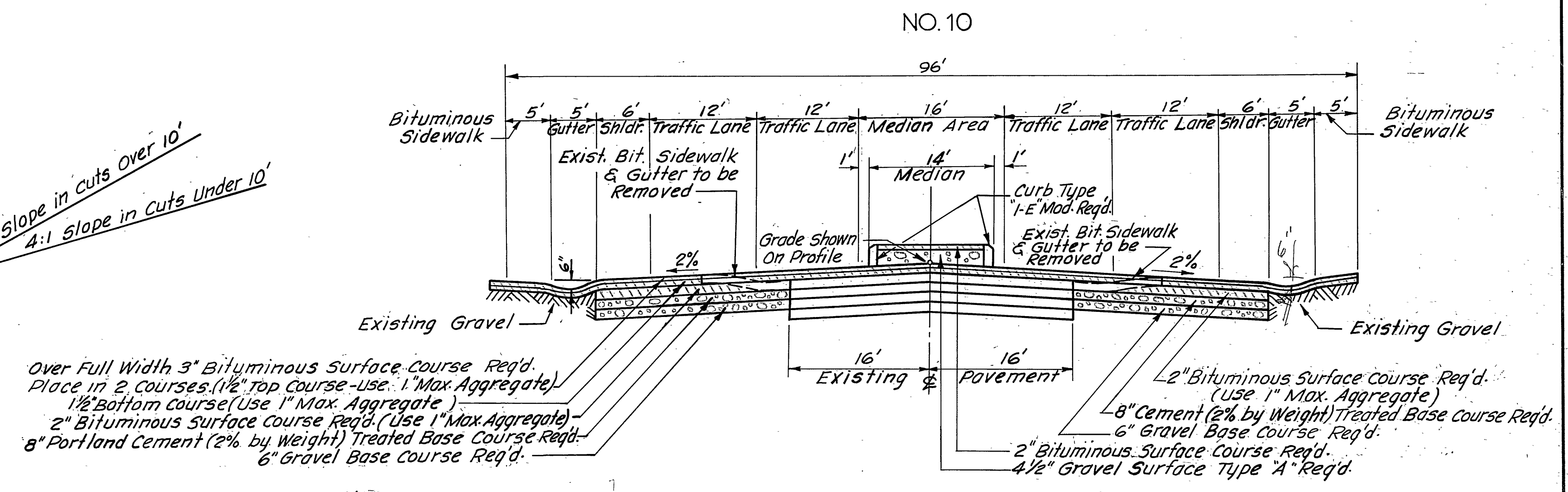
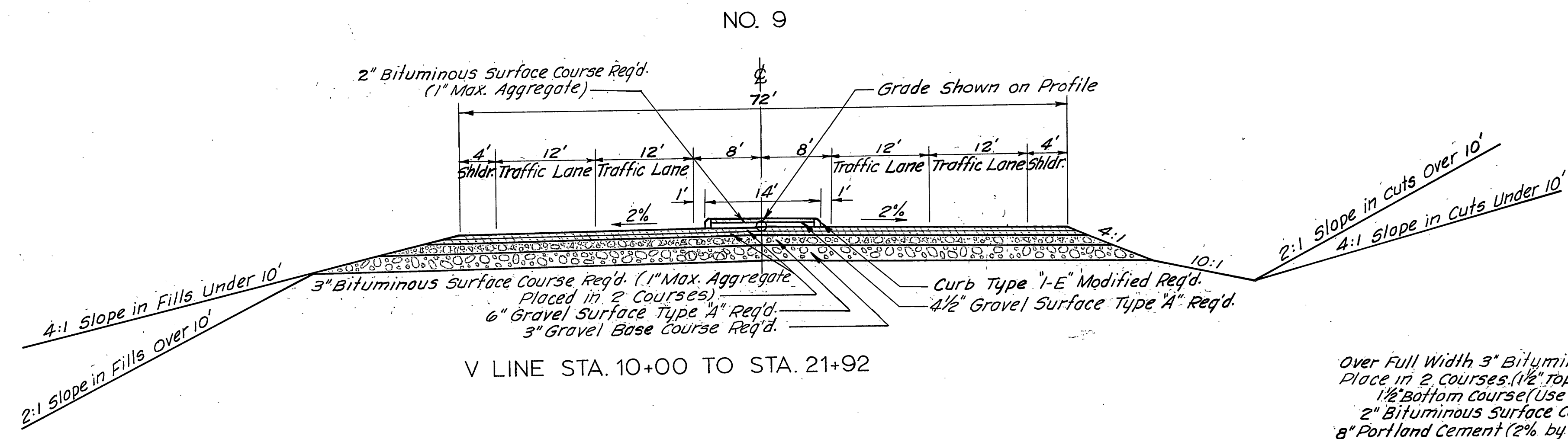


NO. 8  
 RIVERDALE ROAD AT R-2 AND F-5A LINES

| REVISIONS | DATE | BY |
|-----------|------|----|
|           |      |    |
|           |      |    |
|           |      |    |

# TYPICAL SECTIONS

| FED. ROAD DIST. NO. | STATE | PROJ. NO.    | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|--------------|-------------|-----------|--------------|
| UTAH                | UTAH  | 15 7(58) 332 |             | 2-8       |              |
|                     |       | 15 8(20) 334 |             |           |              |
| 16 15 B(18) 334     |       |              |             |           |              |
| Rev. 9-16-63        |       |              |             |           |              |
| Rev. 9-18-63        |       |              |             |           |              |



Over Full Width 3" Bituminous Surface Course Req'd. Place in 2 Courses (1/2" top course - use 1" Max. Aggregate) 1/2" bottom course (use 1" Max. Aggregate)

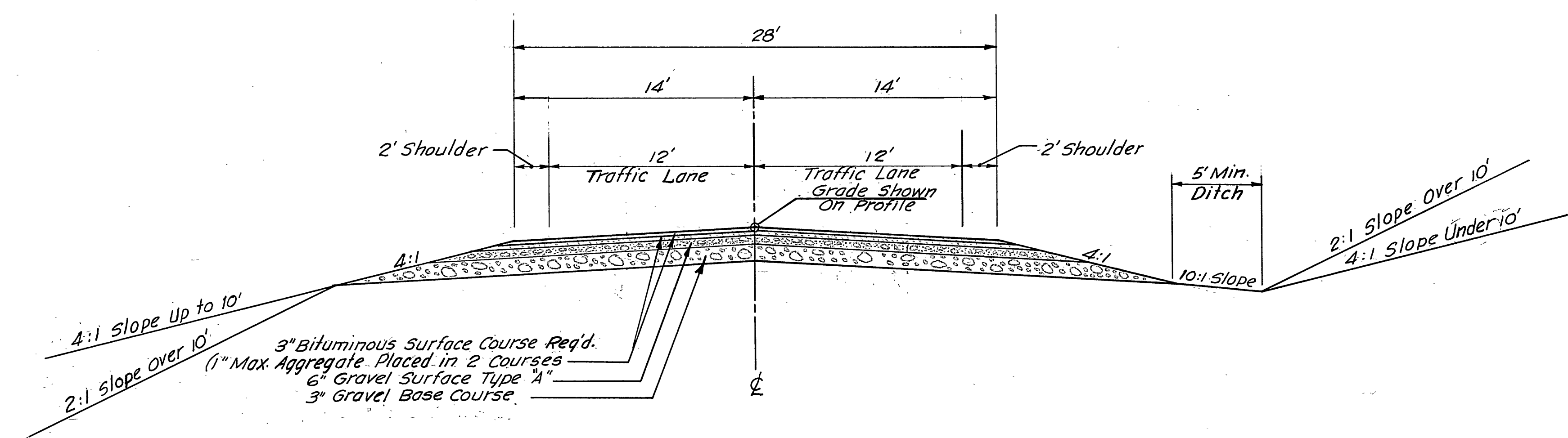
2" Bituminous Surface Course Req'd. (Use 1" Max. Aggregate)

8" Portland Cement (2% by Weight) Treated Base Course Req'd.

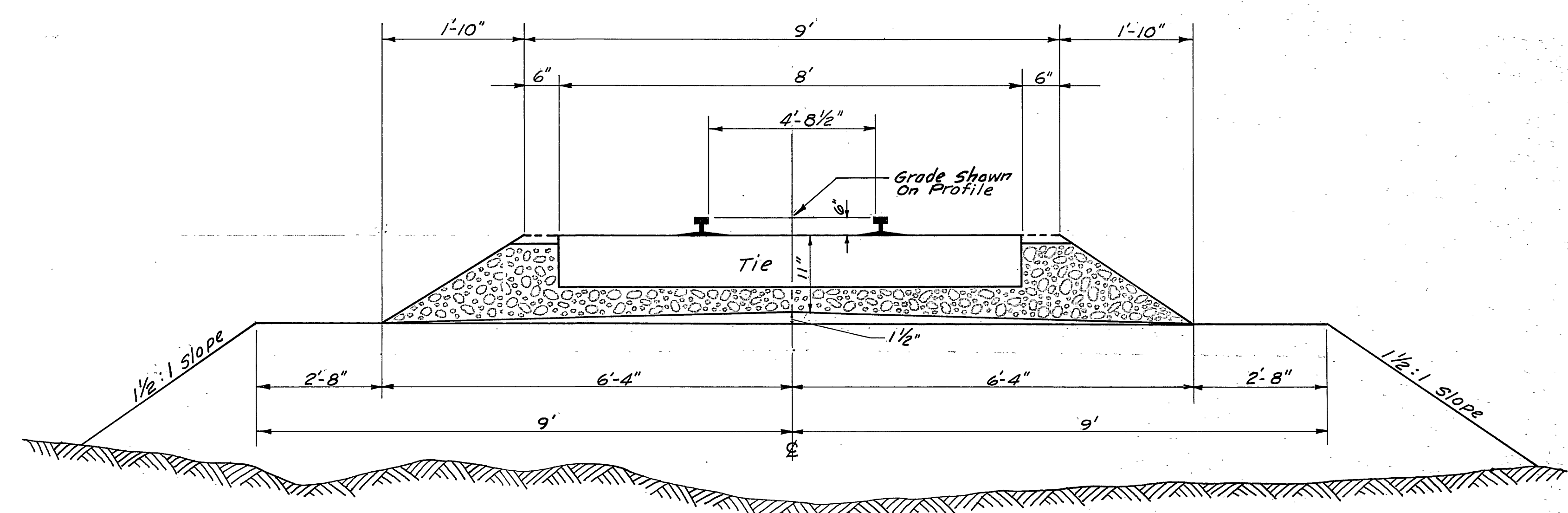
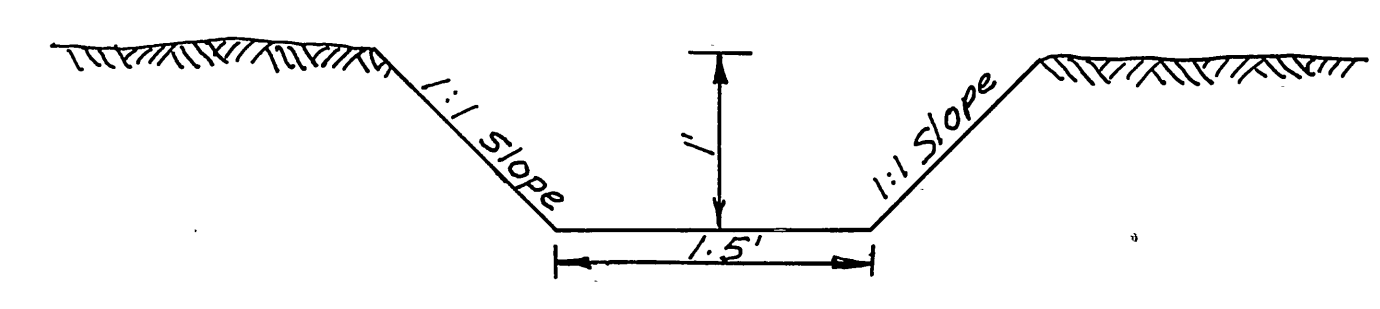
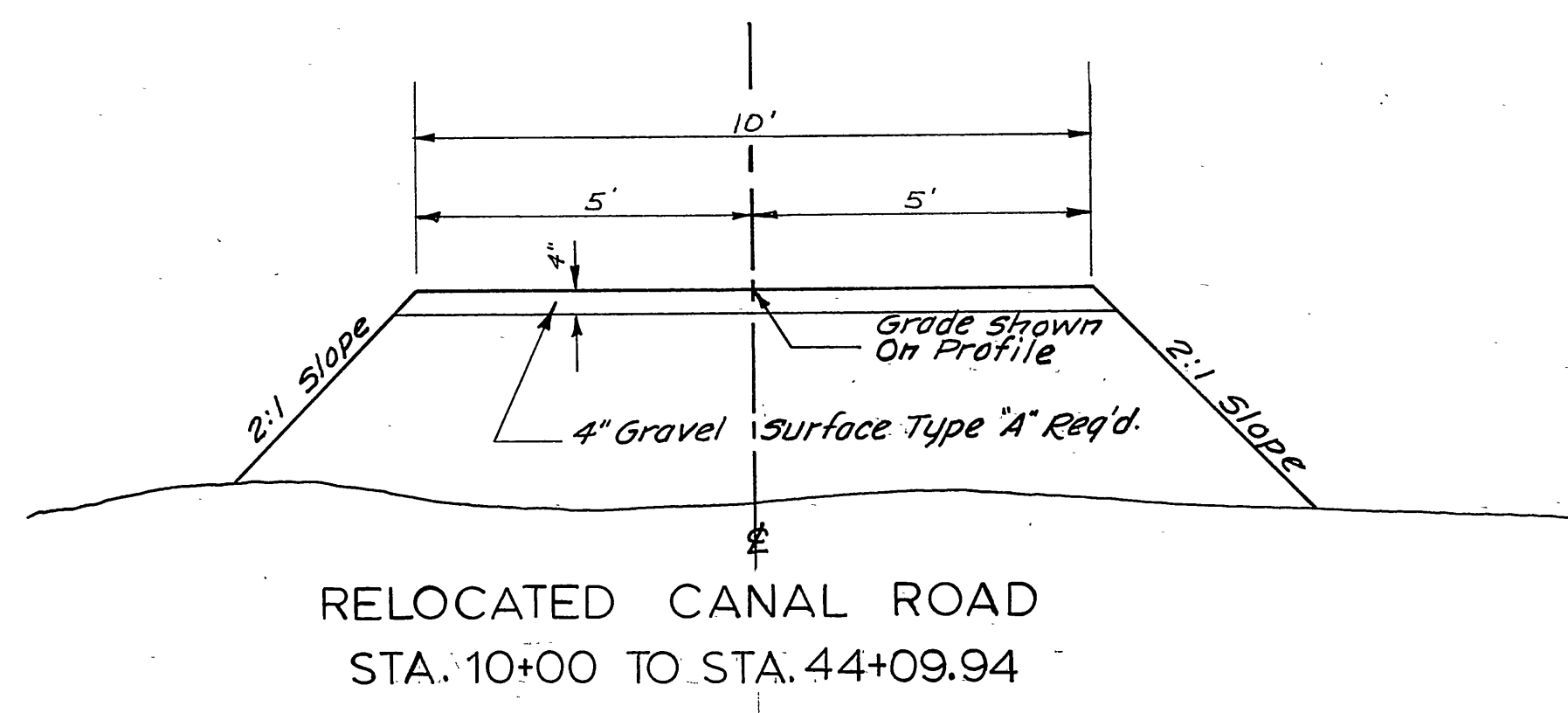
6" Gravel Base course Req'd.

NOTE: Shaping of Rolled Gutter will Not be Paid for Separately But Will be Included in Other Bid Items of Work.

| REVISIONS | BY | DATE |
|-----------|----|------|
|           |    |      |
|           |    |      |
|           |    |      |



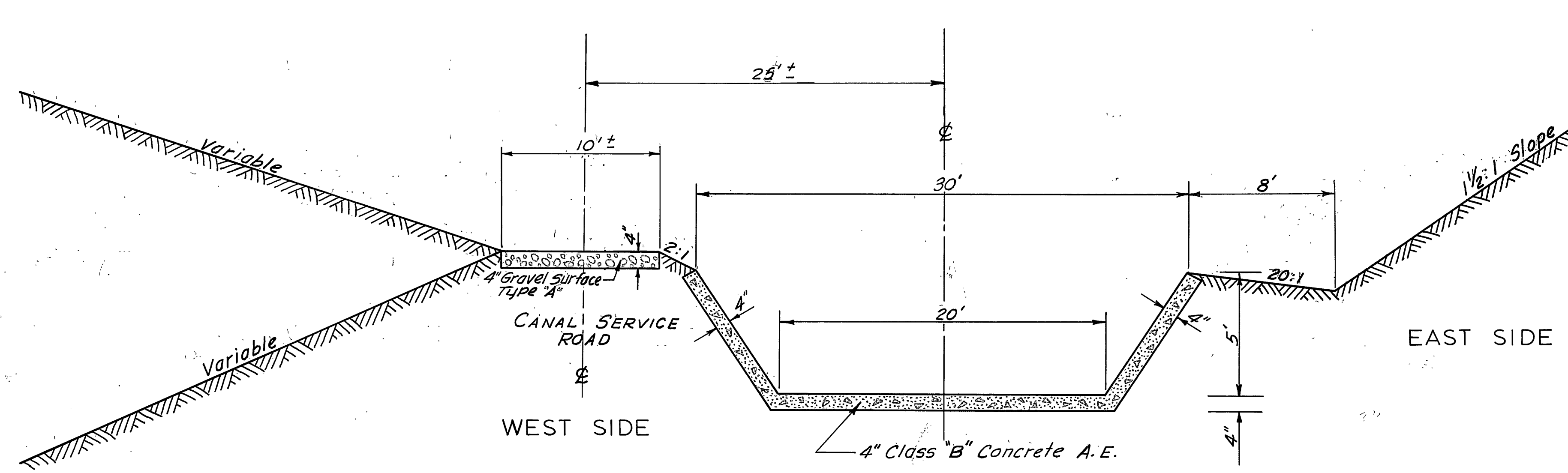
R-7 LINE STA. 10+00 TO STA. 21+41.04  
 R-9 LINE STA. 10+00 TO STA. 22+42.63  
 R-10 LINE STA. 10+00 TO STA. 19+54.17  
 R-8 LINE STA. 10+00 TO STA. 22+94.38



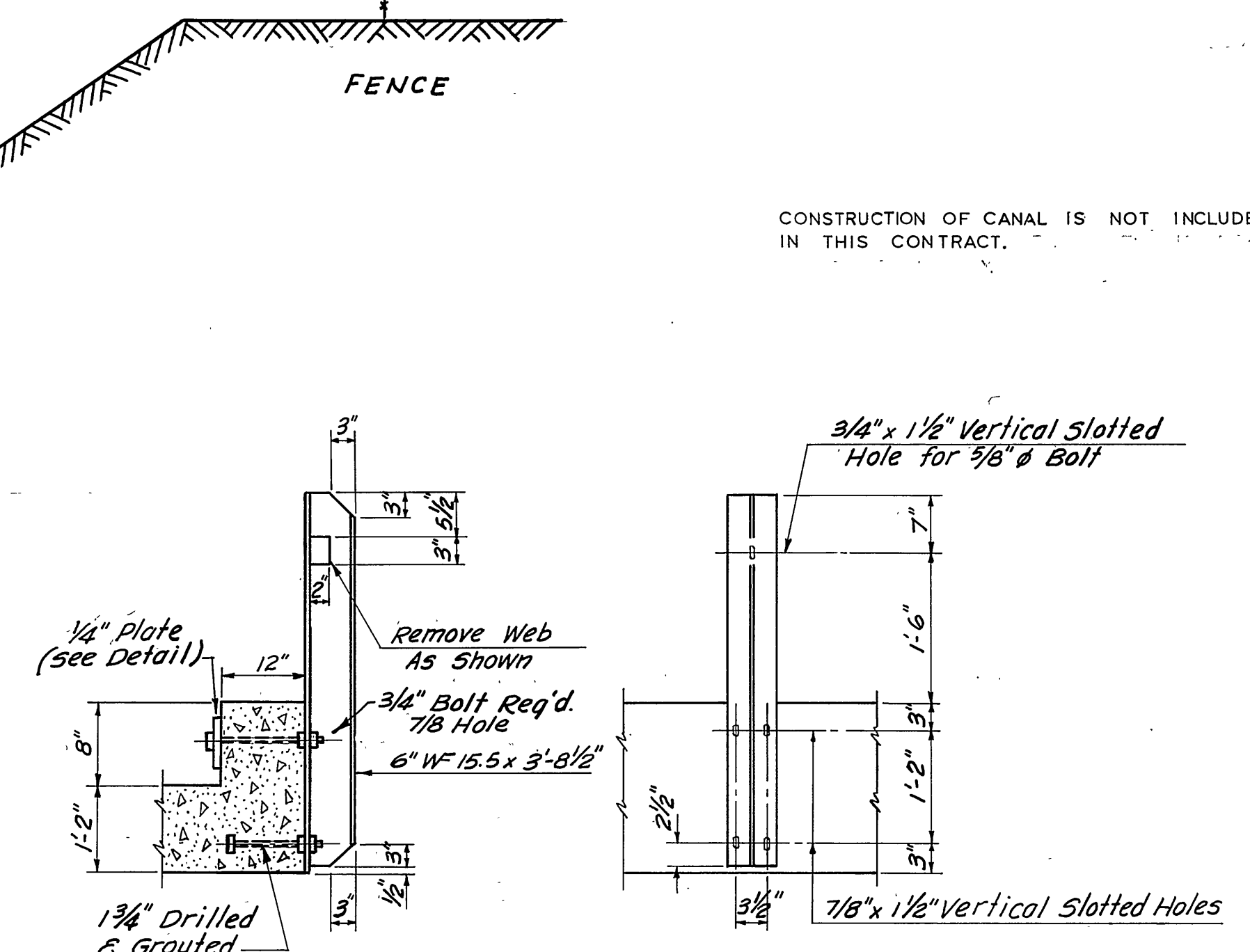
U.P.R.R. RELOCATION  
 STA. 10+06.62 TO STA. 28+67.14  
 (GRADING ONLY)

NOTE: Ballast Ties and Rails to be Constructed By Others.

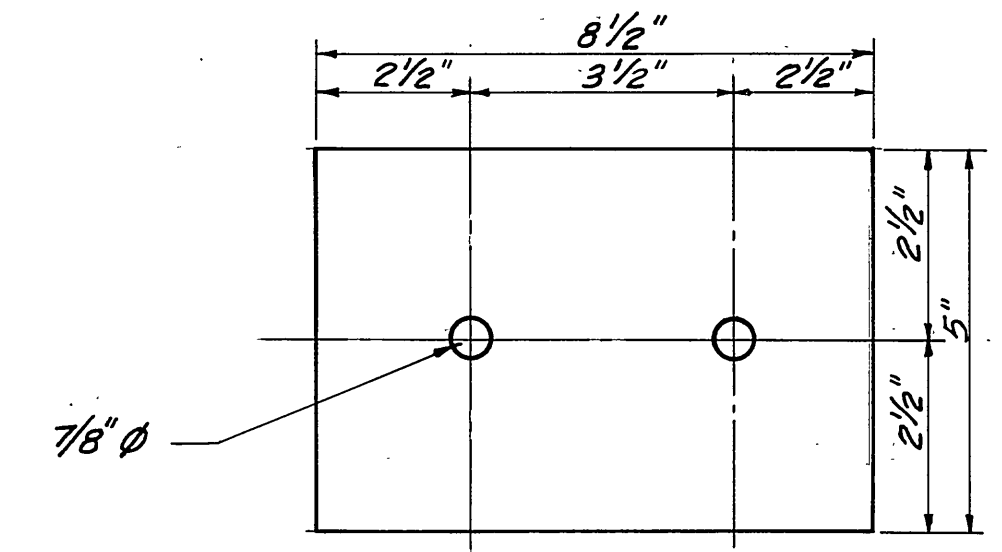
| FED. ROAD DIST. NO. | STATE | PROJ. NO.      | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|----------------|-------------|-----------|--------------|
| UTAH                | UTAH  | 115 7 (58) 332 |             | 2         | 5            |
|                     |       | 115 8 (20) 334 |             |           |              |
|                     |       | 115 8 (18) 334 |             |           |              |
| REV. 9-16-63        |       |                |             |           |              |



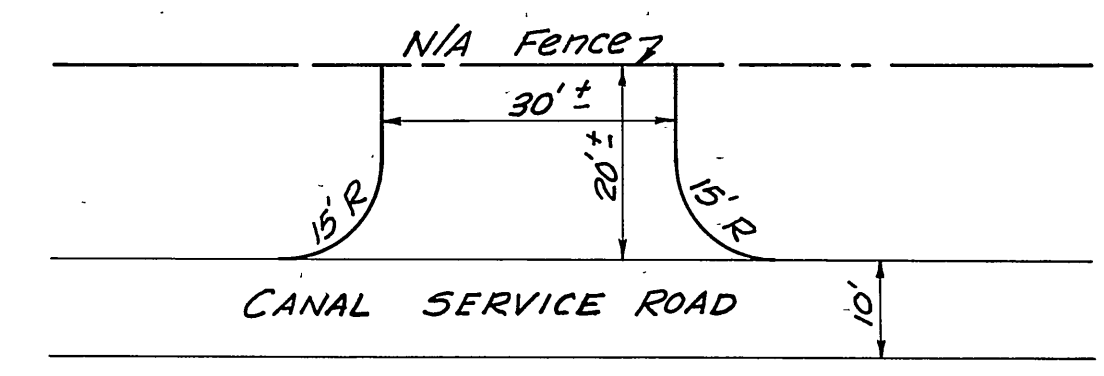
CROSS SECTION — DAVIS WEBER CANAL  
 STA. 10+00 TO STA. 35+66.58  
 NORTHWEST CORNER OF HILL FIELD  
 I-15-8 (12) 334



GUARDRAIL POST  
 (2 Required)  
 "V" LINE

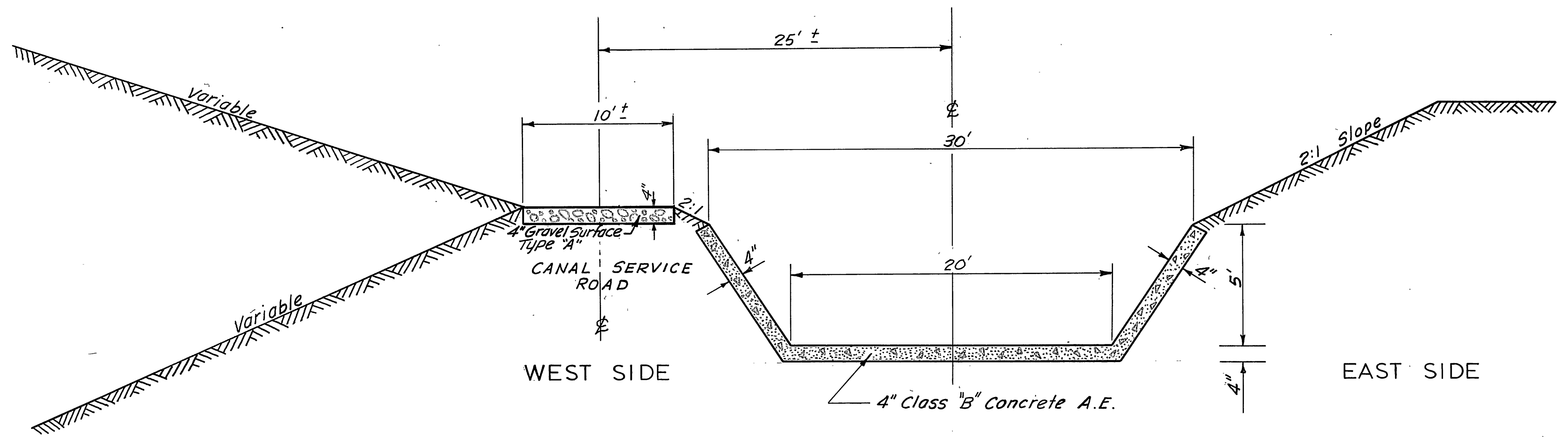


1/4" PLATE



RELOCATED CANAL

R.R. Fence  
 Provide Maintenance Turn-Arounds  
 As Shown or as Directed by the Project  
 Engineer.  
 Place at Station 785 ± and Station 820 ±.



CROSS SECTION — DAVIS WEBER CANAL  
 STA. 13+50 TO STA. 66+40.77

| REVISIONS | BY | DATE |
|-----------|----|------|
|           |    |      |
|           |    |      |
|           |    |      |

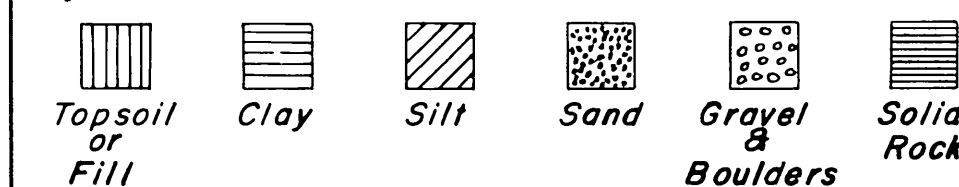


**NOTES**

The information on these materials prospects shall in no way be construed so as to conflict with "Division I, Section 1-6.2 of Designated Local Materials Sources of the State of Utah Standard Specifications, 1960 Edition and Supplements."

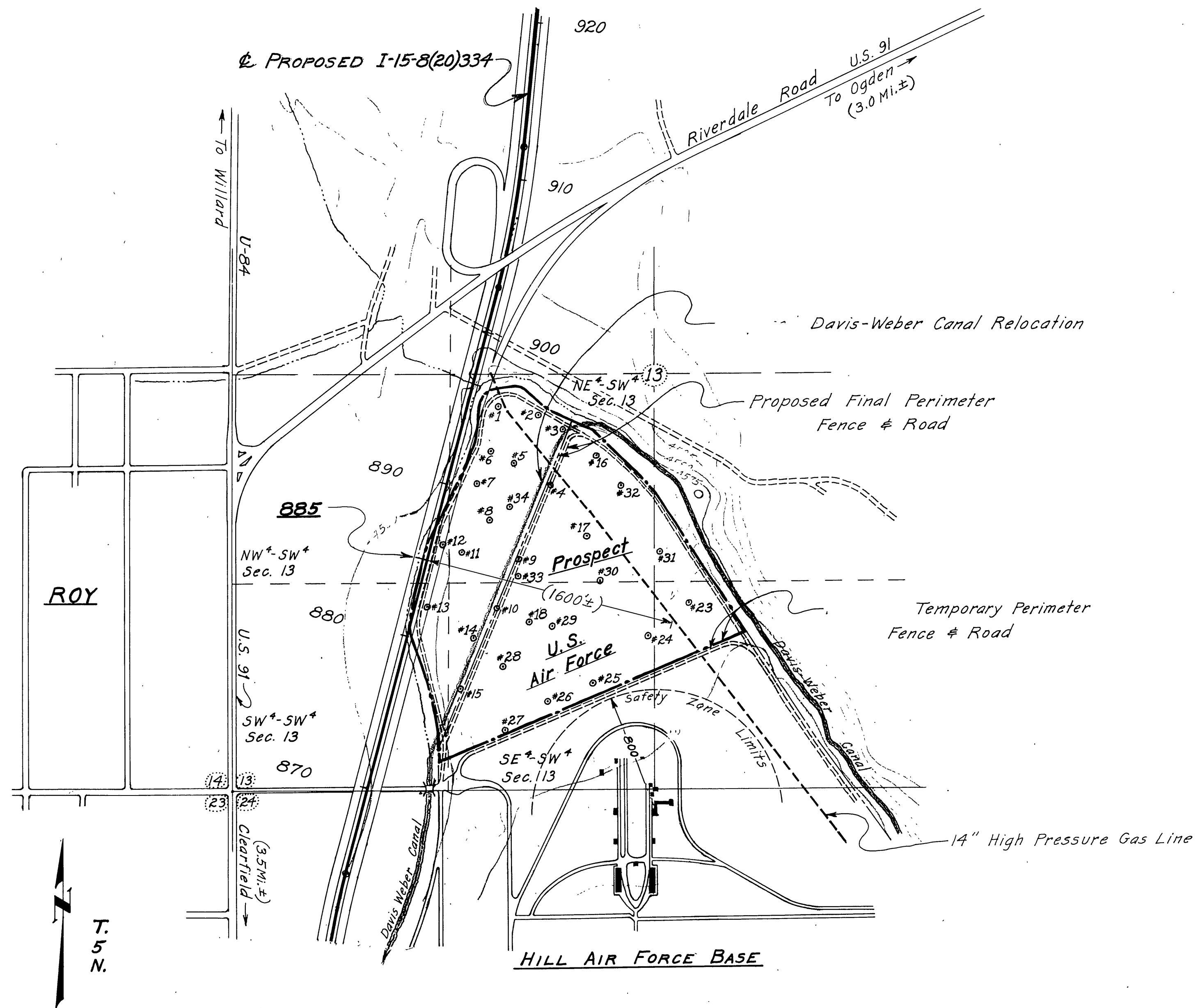
**LEGEND**

Ground Water Table (G.W.T.) = ▽, ○ = Test Hole  
 Bottom of Test Hole = \* , ○ = Section No.  
 Section Line ( § ) = \_\_\_\_\_  
 1/4 Section Line = \_\_\_\_\_  
 1/16 Section Line = \_\_\_\_\_  
 Proposed Highway = \_\_\_\_\_  
 Prospect Boundary = \_\_\_\_\_  
 Property Boundary = \_\_\_\_\_



|   |       |  |             |                  |              |
|---|-------|--|-------------|------------------|--------------|
| FED. ROAD DIV. NO.  | STATE | FEDERAL AID PROJECT NO.                          | FISCAL YEAR | SHEET NO.        | TOTAL SHEETS |
| UTAH  | UTAH  | 1-15-7(58)332<br>1-15-8(20)334<br>16-15-4(10)334 |             | 3                |              |
| <b>UTAH STATE DEPARTMENT OF HIGHWAYS MATERIALS SECTION</b>  |       |  |             |                  |              |
| <b>MATERIALS PROSPECTS AND TEST HOLES</b>   |       |  |             |                  |              |
| PROJECT NO. I-15-7(58)332 # I-15-8(20)334   |       |  |             |                  |              |
| PROJECT NAME SOUTH. OF HILL AIRBASE INTERCHANGE TO WEBER-DAVIS COUNTY LINE TO 4400 SOUTH IN RIVERDALE |       |  |             |                  |              |
| COUNTY DAVIS & WEBER  |       |  |             |                  |              |
| DATE: MAY, 1963. DRAWN BY: W. LOFTON  |       |  |             |                  |              |
| DATA BY: AUTH: 5854, 5850   |       |  |             |                  |              |
| FIELD BOOK:   |       |  |             | SHEET NO. 1 OF 1 |              |

REV 8-30-63

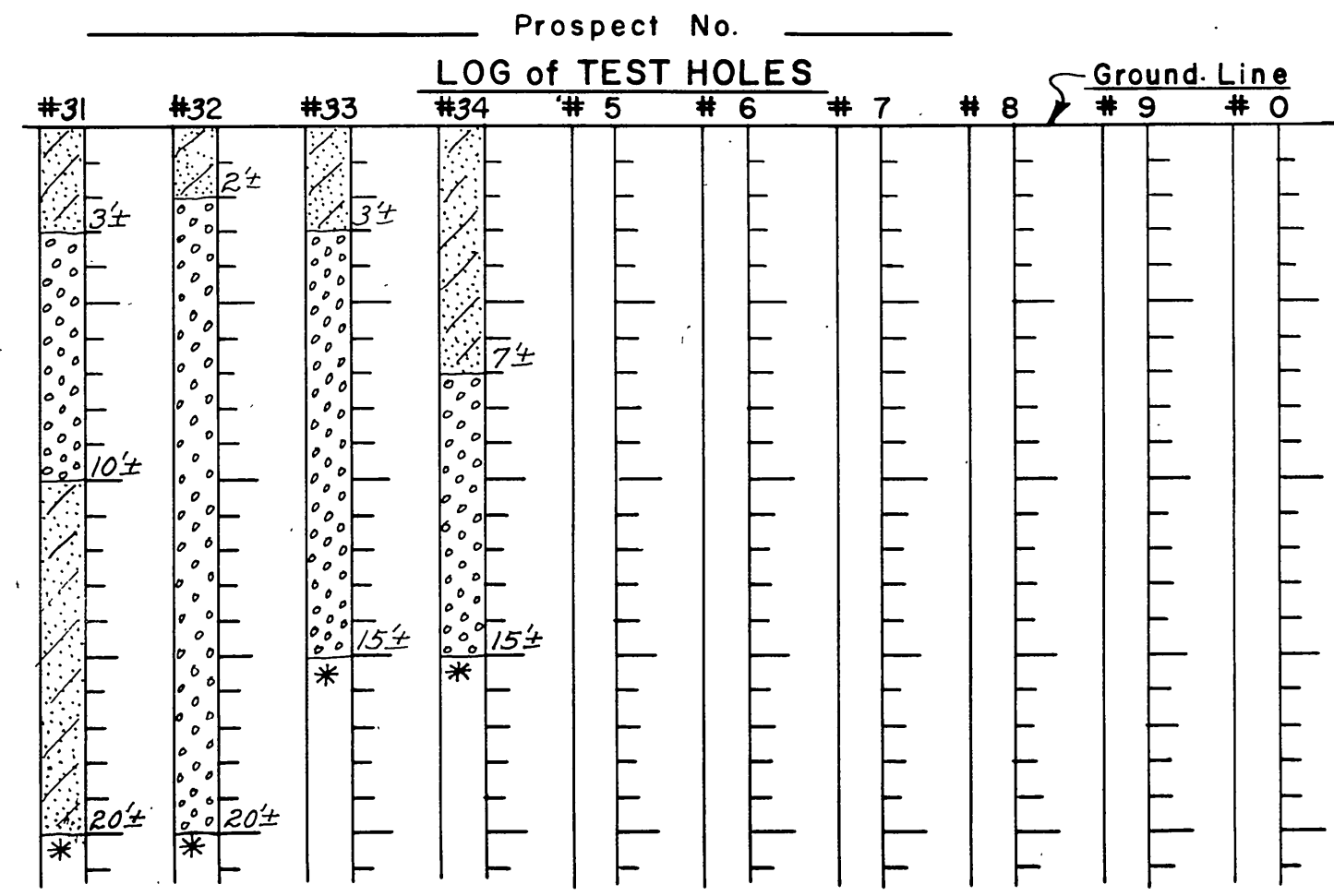
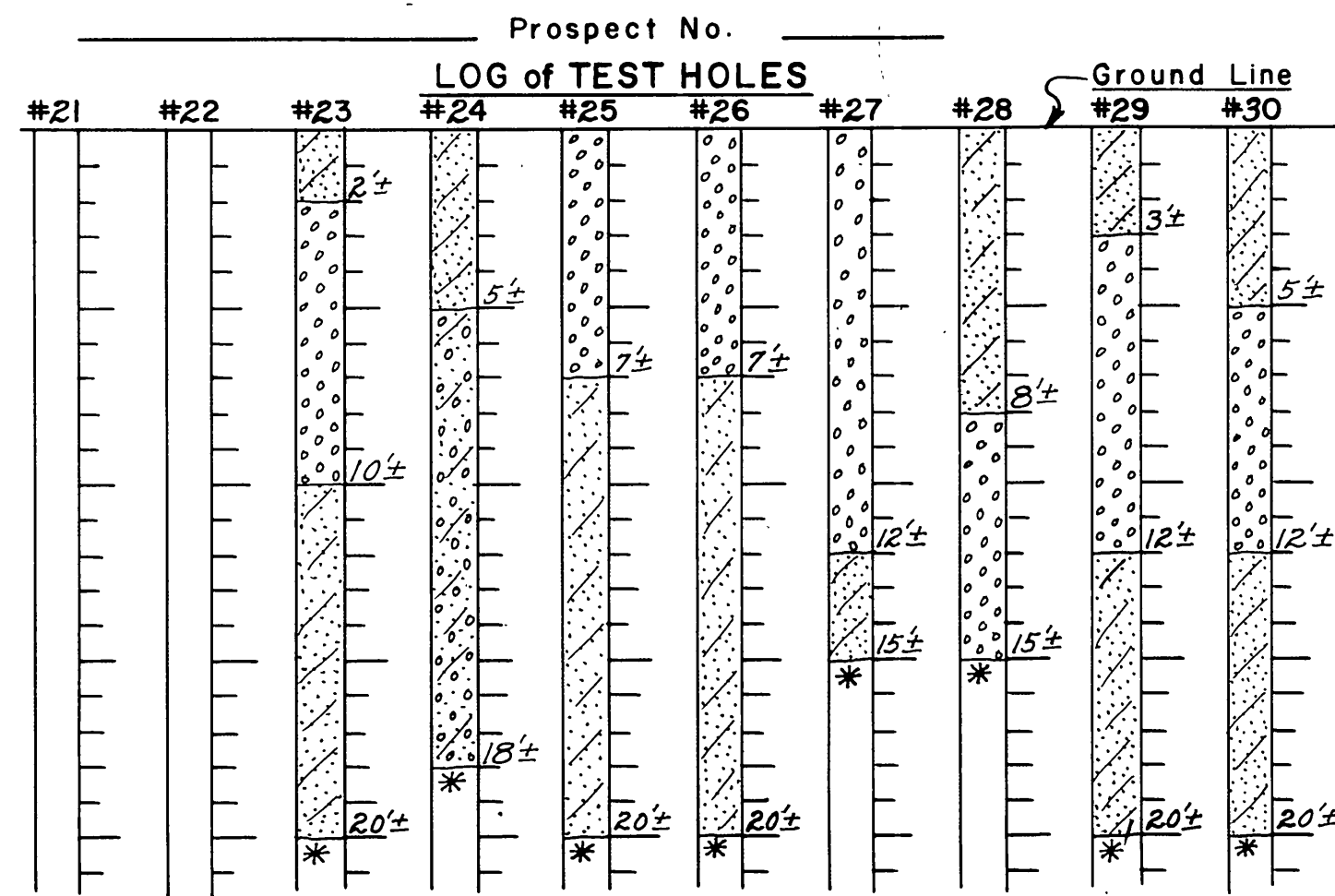
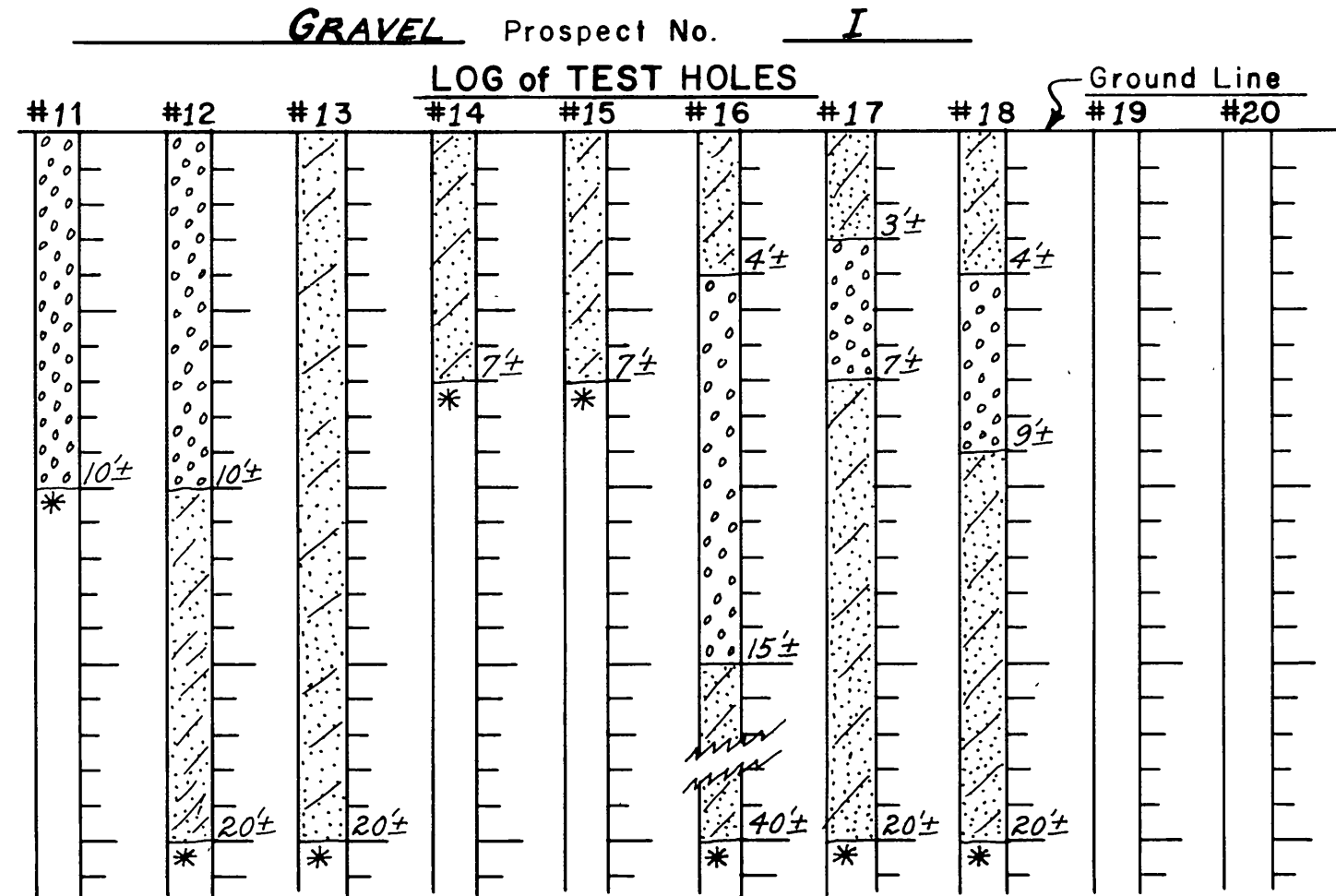
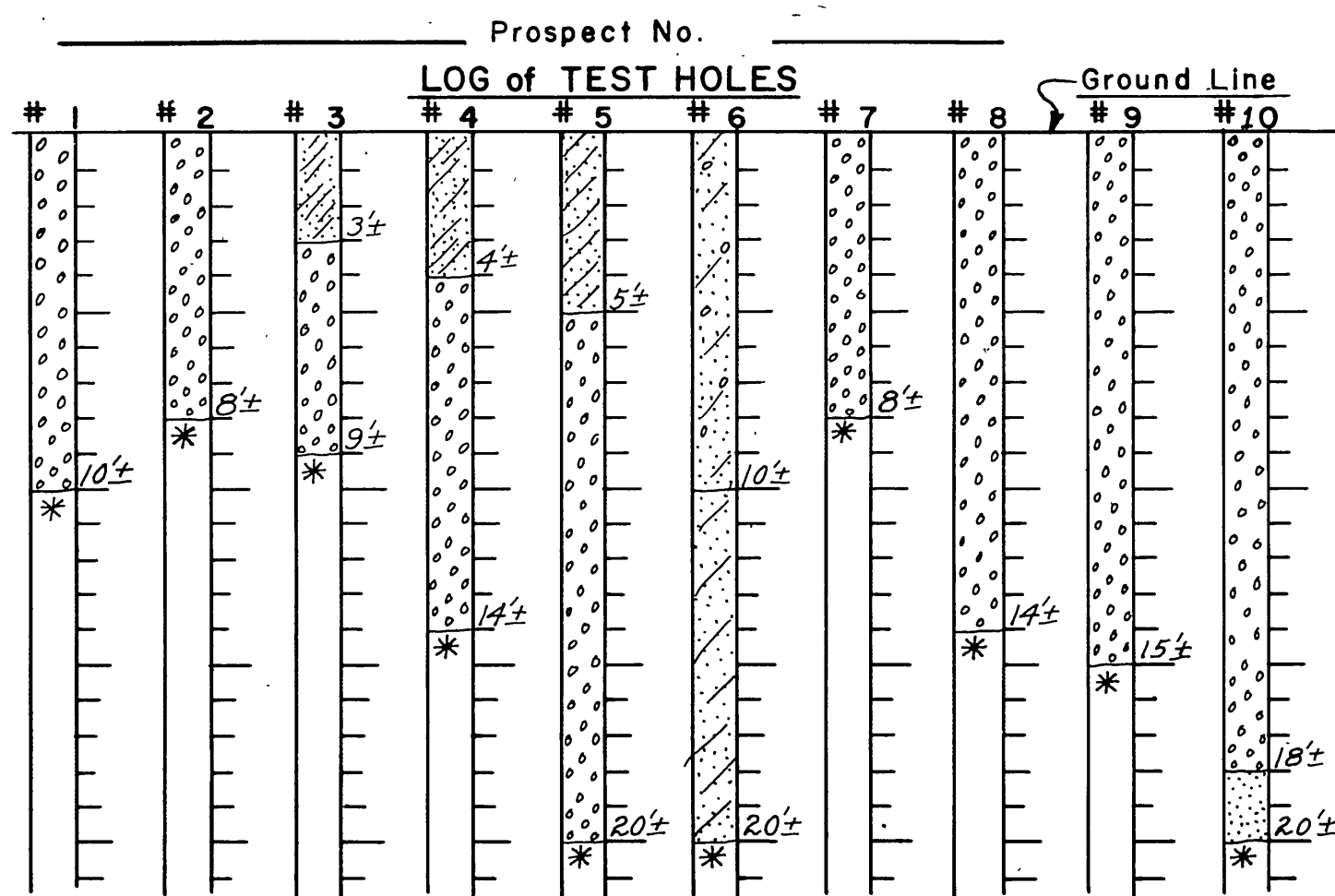


B. & M. Scale: 1" = \_\_\_\_\_ Contour Interval = \_\_\_\_\_

R-2-W. S.L.B. & M. Scale: 1" = 500' Contour Interval = \_\_\_\_\_

B. & M. Scale: 1" = \_\_\_\_\_ Contour Interval = \_\_\_\_\_

B. & M. Scale: 1" = \_\_\_\_\_ Contour Interval = \_\_\_\_\_



# SUMMARY SHEET

NOTE: FOR CONSTRUCTION CHANGES SUMMARY SHEET  
SEE BASIC CONSTRUCTION PLANS NO D-25

I-15-7(58)332  
I-15-8(20)334  
I-15-8(18)334  
Rev 8-30-63  
REV 9-16-63

3-A

| PROJECT         | LOCATION         | STATION  |          | LENGTH<br>LINEAR FEET | GRAVEL MATERIAL                  |              |                                     |              |                         |                           |                      | BITUMINOUS MATERIAL          |                   |       |  |                                    |                                    |                                     |      |      |      |      |
|-----------------|------------------|----------|----------|-----------------------|----------------------------------|--------------|-------------------------------------|--------------|-------------------------|---------------------------|----------------------|------------------------------|-------------------|-------|--|------------------------------------|------------------------------------|-------------------------------------|------|------|------|------|
|                 |                  | FROM     | TO       |                       | BITUMINOUS SURFACE COURSE (TONS) |              | GRAVEL SURFACE COURSE TYPE A (TONS) |              | GRAVEL BASE COURSE TONS | PC TREAT BASE COURSE TONS | PORTLAND CEMENT BAGS | COVER MATERIAL TYPE "A" TONS | 60-70 PENETRATION |       | MC-400 FRONTAGE & CROSSROADS 5 GAL/YD <sup>2</sup> | HYDRATED LIME 1% OF BIT SUR (TONS) | RC-70 TONS .15 GAL/YD <sup>2</sup> | RC-800 TONS .25 GAL/YD <sup>2</sup> |      |      |      |      |
|                 |                  |          |          |                       | MEDIAN                           |              | ROADWAY                             |              |                         |                           |                      |                              | TONS              | TONS  |  |                                    |                                    |                                     | TONS | TONS | TONS | TONS |
|                 |                  |          |          |                       | 1" MAX. AGG.                     | 1" MAX. AGG. | 1" MAX. AGG.                        | 1" MAX. AGG. |                         |                           |                      |                              |                   |       |  |                                    |                                    |                                     |      |      |      |      |
| I-15-7(58) 332  | "V"              | 10+00    | 21+92    | 1192.00               | 82                               | 1427         | 170                                 | 2360         | 1439                    |                           |                      | 104.9                        | 78                | 18.3  | 15.1   | 5.4                                | 8.9                                |                                     |      |      |      |      |
| SUB TOTAL       |                  |          |          |                       | 82                               | 1427         | 170                                 | 2360         | 1439                    |                           |                      | 104.9                        | 78                | 18.3  | 15.1   | 5.4                                | 8.9                                |                                     |      |      |      |      |
| TOTAL           |                  |          |          |                       | 1509                             |              | 2530                                | 1439         |                         |                           | 104.9                | 78                           | 18.3              | 15.1  | 5.4  | 8.9                                |                                    |                                     |      |      |      |      |
| USE             |                  |          |          |                       | 1590                             |              | 2660                                | 1600         |                         |                           | 110                  | 90                           | 20.0              | 16    | 10   | 10.0                               |                                    |                                     |      |      |      |      |
| I-15-8 (20) 334 | "F-5"            | 10+00    | 15+21.53 | 521.53                | 81.3                             | 446          | 168.4                               | 876          | 465                     |                           |                      | 31.9                         | 25                | 5.7   | 5.3  | 1.6                                | 2.7                                |                                     |      |      |      |      |
| I-15-8 (20) 334 | "F-5"            | 15+21.53 | 20+54.78 | 533.25                | 41.9                             | 375          | 86.8                                | 747          | 401                     |                           |                      | 26.7                         | 21                | 4.8   | 5.2  | 1.4                                | 2.3                                |                                     |      |      |      |      |
| I-15-8 (20) 334 | "F-5"            | 20+54.78 | 57+08.35 | 3653.57               |                                  | 2013         |                                     | 4092         | 2238                    |                           |                      | 142.1                        | 111               | 25.9  | 20.1   | 7.5                                | 12.1                               |                                     |      |      |      |      |
| I-15-8 (20) 334 | "F-5A"           | 10+00    | 12+87    | 287.00                |                                  | 121          |                                     | 29           | 268                     | 295                       | 126                  | 6.4                          | 8                 | 0.4   | 1.5  | 0.7                                | 0.5                                |                                     |      |      |      |      |
| I-15-8 (20) 334 | "F-5A" Riverdale |          |          | 953.00                |                                  | 482          |                                     | 110          | 957                     | 1067                      | 454                  | 23.8                         | 33                | 1.4   | 5.9  | 2.6                                | 2.0                                |                                     |      |      |      |      |
| I-15-8 (20) 334 | "R-2" Riverdale  |          |          | 500.00                |                                  | 279          |                                     | 64           | 537                     | 607                       | 258                  | 13.9                         | 19                | 0.8   | 3.4  | 1.5                                | 1.2                                |                                     |      |      |      |      |
| I-15-8 (20) 334 | "R-7"            | 10+00    | 21+41.04 | 1141.04               |                                  | 629          |                                     |              | 1279                    | 699                       |                      | 44.3                         | 35                | 8.1   | 6.3  | 2.3                                | 3.8                                |                                     |      |      |      |      |
| I-15-8 (20) 334 | "R-8"            | 10+00    | 22+94.38 | 1294.38               |                                  | 713          |                                     |              | 1450                    | 793                       |                      | 50.3                         | 39                | 9.2   | 7.1  | 2.7                                | 4.3                                |                                     |      |      |      |      |
| I-15-8 (20) 334 | "R-9"            | 10+00    | 22+42.63 | 1242.63               |                                  | 685          |                                     |              | 1391                    | 761                       |                      | 48.3                         | 38                | 8.8   | 6.9  | 2.5                                | 4.1                                |                                     |      |      |      |      |
| I-15-8 (20) 334 | "R-10"           | 10+00    | 19+54.17 | 954.17                |                                  | 526          |                                     |              | 1069                    | 584                       |                      | 37.1                         | 29                | 6.8   | 5.3  | 2.0                                | 3.2                                |                                     |      |      |      |      |
| I-15-8 (20) 334 | 5600 So.         | 4+20     | 13+20    | 900                   | 57                               | 1595         | 547                                 | 120          | 1386                    | 1848                      | 786                  | 75.0                         | 95                | * 4.8 | 16.5   | 8.9                                | 6.4                                |                                     |      |      |      |      |
| Canal Road      |                  | 10+00    | 44+09    | 3409                  |                                  |              |                                     |              | 600                     |                           |                      |                              |                   |       |  |                                    |                                    |                                     |      |      |      |      |
| Canal Road      | Canal Reloc.     | 13+50    | 66+40    | 52+90                 |                                  |              |                                     |              | 1234                    |                           |                      |                              |                   |       |  |                                    |                                    |                                     |      |      |      |      |
| Canal Road      | Canal Reloc.     | 10+00    | 35+67    | 25+67                 |                                  |              |                                     |              | 599                     |                           |                      |                              |                   |       |  |                                    |                                    |                                     |      |      |      |      |
| SUB TOTAL       |                  |          |          |                       | 180.2                            | 7864         | 777                                 | 375          | 13,331                  | 9089                      | 3817                 | 1624                         | 500               | 453   | 71.9   | 83.5                               | 33.7                               | 42.6                                |      |      |      |      |
| TOTAL           |                  |          |          |                       | 8044                             |              | 777                                 |              | 13,712                  | 9089                      | 3817                 | 1624                         | 500               | 453   | 71.9   | 83.5                               | 33.7                               | 42.6                                |      |      |      |      |
| USE             |                  |          |          |                       |                                  | 9265         |                                     |              | 14,400                  | 9600                      | 4000                 | 1700                         | 530               | 480   | 90   | 90                                 | 40                                 | 50                                  |      |      |      |      |
| GRAND TOTAL     |                  |          |          |                       |                                  | 10,855       |                                     |              | 17,060                  | 11,200                    | 4000                 | 1700                         | 640               | 570   | 110  | 106                                | 50                                 | 60                                  |      |      |      |      |

| REMARKS<br>PROJECT NUMBER                      | LINE           | STATION    |           | MAIN LINE |       | RAMP ROAD | CROSS ROAD | FRONTAGE ROAD |
|--|----------------|------------|-----------|-----------|-------|-----------|------------|---------------|
|  |                | FROM       | TO        | FEET      | MILES |           |            |               |
| I-15-7(58) 332<br>(Sta. 700+00 to Sta. 836+15) |                |            |           |           |       |           |            |               |
| Roadway  | I-15           | 700+00     | 728+83.39 | 2883.39   | 0.546 |           |            |               |
| Hill Field Structure                           |                | 728+83.39  | 730+42.05 | 158.66    | 0.030 |           |            |               |
| Roadway  | I-15           | 730+42.05  | 836+15.00 | 10,605.42 | 2.009 |           |            |               |
| Equation                                       |                | 750+32.478 | 950+00.00 |           |       |           |            |               |
| Roadway  | "T"            | 10+00      | 41+30.03  |           |       | 3130.03   |            |               |
| Roadway  | "U"            | 10+00      | 41+67.29  |           |       | 3167.29   |            |               |
| Roadway  | "V"            | 10+00      | 21+92.00  |           |       |           | 1192.0     |               |
| Construct Median Only                          | "W"            | 275+92.50  | 280+52.50 |           |       |           | 460.0      |               |
| Construct Median Only                          | "W"            | 281+76.00  | 287+86.00 |           |       |           | 610.0      |               |
| SUB TOTALS                                     |                |            |           |           |       |           |            |               |
| ROADWAY TOTAL                                  |                |            |           | 13,488.81 | 2.555 | 6297.32   | 2262       |               |
| STRUCTURE TOTAL                                |                |            |           | 158.67    | 0.030 |           |            |               |
| PROJECT TOTAL                                  |                |            |           | 13,647.48 | 2.585 | 6297.32   | 2262       |               |
| I-15-8 (20) 334<br>Sta. 836+15 to Sta. 843+00  |                |            |           |           |       |           |            |               |
| Roadway  |                | 836+15.00  | 843+00.00 | 685.00    | 0.130 |           |            |               |
| IG-15-8 (18) 334<br>Sta. 843+00 to Sta. 863+00 |                |            |           |           |       |           |            |               |
| Roadway  | I-15           | 843+00.00  | 853+64.88 | 1071.26   | 0.203 |           |            |               |
| Equation                                       |                | 845+15.108 | 845+08.72 |           |       |           |            |               |
| Railroad Overpass                              | I-15           | 853+64.88  | 855+75.12 | 210.24    | 0.040 |           |            |               |
| Roadway  | I-15           | 855+75.12  | 863+00.00 | 724.88    | 0.137 |           |            |               |
| ROADWAY TOTAL                                  |                |            |           | 1796.14   | 0.340 |           |            |               |
| STRUCTURE TOTAL                                |                |            |           | 210.24    | 0.040 |           |            |               |
| I-15-8 (20) 334<br>Sta. 863+00 to Sta. 894+00  |                |            |           |           |       |           |            |               |
| Roadway  | I-15           | 863+00.00  | 868+95.97 | 595.57    | 0.113 |           |            |               |
| 5600 So. Structure                             | I-15           | 868+95.97  | 870+79.03 | 183.46    | 0.035 |           |            |               |
| Roadway  | I-15           | 870+79.03  | 964+00    | 9320.97   | 1.765 |           |            |               |
| Roadway  | "F-1"          | 10+00.00   | 19+51.34  |           |       | 951.34    |            |               |
| Roadway  | "F-2"          | 10+00.00   | 20+75.13  |           |       | 1075.13   |            |               |
| Roadway  | "F-3"          | 10+00.00   | 24+55.00  |           |       | 1455.00   |            |               |
| Roadway  | "F-4"          | 10+00.00   | 20+57.88  |           |       | 1057.88   |            |               |
| Widening & Median                              | 5600 So.       | 4+17.6     | 13+20.00  |           |       | 902.4     |            |               |
| Roadway  | "F-5"          | 10+00.00   | 57+08.35  |           |       | 4708.35   |            |               |
| Roadway  | "F-5A"         | 10+00.00   | 12+87.00  |           |       |           |            |               |
| Structure                                      | Riverdale Road | 28+40.01   | 31+85.06  |           |       | 345.02    |            |               |
| Widening & Median                              | Riverdale Road | 31+85.06   | 54+22.00  |           |       | 2236.94   |            |               |
| Roadway  | "R-1"          | 10+00.00   | 23+33.96  |           |       | 1333.96   |            |               |
| Roadway  | "R-2"          | 10+00.00   | 17+90.07  |           |       | 790.07    |            |               |
| Roadway  | "R-7"          | 10+00      | 25+60.71  |           |       | 1560.71   |            |               |
| Roadway  | "R-10"         | 10+00.00   | 19+54.17  |           |       | 954.17    |            |               |
| Roadway  | "R-9"          | 10+00      | 18+32.09  |           |       | 832.09    |            |               |
| structure                                      | "R-9"          | 18+32.09   | 20+71.21  |           |       | 239.12    |            |               |
| Roadway  | "R-9"          | 20+71.21   | 23+00.00  |           |       | 228.79    |            |               |
| ROADWAY TOTAL                                  |                |            |           | 9916.54   | 2.008 | 6663.38   | 4784.36    | 7223.23       |
| STRUCTURE TOTAL                                |                |            |           | 183.46    | 0.035 |           |            |               |
| PROJECT TOTAL                                  |                |            |           | 10,100.00 | 2.043 |           |            |               |
| R.R. Relocation (Not Included in Grand Total)  | R.R.           |            |           | 1860.52   | 0.352 |           |            |               |
| CONTRACT TOTALS                                |                |            |           |           |       |           |            |               |
| ROADWAY TOTAL                                  |                |            |           | 25,886.49 | 4.903 | 12,960.70 | 7046.36    | 7223.23       |
| STRUCTURE TOTAL                                |                |            |           | 552.37    | 0.105 |           |            |               |
| GRAND TOTAL                                    |                |            |           | 26,438.86 | 5.008 | 12,960.70 | 7046.36    | 7223.23       |

| DEEP BEAM GUARD RAIL      |                    |        |       |                    |
|---------------------------|--------------------|--------|-------|--------------------|
| WOODEN POSTS              |                    |        |       | REMARKS            |
| LINE                      | STATION TO STATION | LT.    | RT.   |                    |
| Main                      | 704+05             | 704+30 | 25    | Use as a Barricade |
| V                         | 13+25              | 14+00  | 75    |                    |
| V                         | 13+25              | 14+00  | 75    |                    |
| SUB TOTAL                 |                    |        | 100   | 75                 |
| 2' For 6 End Sections     |                    |        | 8     | 4                  |
| TOTAL                     |                    |        | 108   | 79                 |
|                           |                    |        | 187   |                    |
| F-5                       | 39+00              | 39+25  | 25.0  |                    |
| Main                      | 918+70             | 918+95 | 25.0  |                    |
| Main                      | 900+65             | 900+93 | 25.0  | Use as a Barricade |
| Main                      | 918+25             | 919+40 | 100.0 | Use as a Barricade |
| Main                      | 924+02             | 924+20 | 25.0  | Use as a Barricade |
| Main                      | 924+02             | 924+20 | 25.0  | Use as a Barricade |
| SUB TOTAL                 |                    |        | 75    | 150                |
| 2 Feet for 6 End Sections |                    |        | 12    |                    |
| 2 Feet for 6 End Sections |                    |        | 12    |                    |
| SUB TOTAL                 |                    |        | 87    | 162                |
| TOTAL                     |                    |        | 249   |                    |
| CONTRACT TOTAL            |                    |        | 436   |                    |

| 6' CHAIN LINK FENCE                |                    |            |      |                                    |                    |            |      |
|------------------------------------|--------------------|------------|------|------------------------------------|--------------------|------------|------|
| LINE                               | STATION TO STATION | LINEAR FT. |      | LINE                               | STATION TO STATION | LINEAR FT. |      |
|                                    |                    | RT.        | LT.  |                                    |                    | RT.        | LT.  |
| M.L.                               | 700+00             | 716+61     | 1661 | R.R.                               | 853+00             | 857+85     | 600  |
| M.L.                               | 700+00             | 714+00     | 1400 | M.L.                               | 853+00             | 863+00     | 983  |
| "T"                                | 10+01              | 23+00      | 1299 | R.R.                               | 857+85             | 863+00     | 540  |
| "V"                                | 10+50              | 14+00      | 350  | IG-15-8 (18) 334 TOTAL 1583 2855   |                    |            |      |
| "V"                                | 10+50              | 13+50      | 300  | M.L.                               | 857+85             | 860+00     | 240  |
| "T"                                | 25+00              | 41+30      | 1630 | "F-1"                              | 13+00              | 19+00      | 600  |
| "U"                                | 10+00              | 24+30      | 1430 | "F-2"                              | 10+50              | 20+36      | 986  |
| "V"                                | 20+50              | 21+80      | 130  | "F-3"                              | 16+00              | 24+75      | 875  |
| "V"                                | 20+50              | 21+80      | 130  | M.L.                               | 871+20             | 900+40     | 2920 |
| "U"                                | 26+00              | 36+25      | 1025 | M.L.                               | 879+92             | 901+00     | 2108 |
| M.L.                               | 746+13             | 836+15     | 9010 | "R-2"                              | 10+00              | 7+90       | 790  |
| M.L.                               | 741+35             | 800+00     | 5885 | Riverdale                          | 35+00              | 43+20      | 800  |
| M.L.                               | 830+00             | 836+15     | 615  | "R-1"                              | 12+25              | 21+60      | 1100 |
| I-15-7(58) 332 TOTAL 10,835 14,030 |                    |            |      | M.L.                               | 911+75             | 964+00     | 5225 |
|                                    |                    |            |      |                                    | 836+15             | 843+00     | 685  |
|                                    |                    |            |      | M.L.                               | 907+75             | 955+00     | 4725 |
|                                    |                    |            |      |                                    | 836+15             | 843+00     | 685  |
|                                    |                    |            |      | M.L.                               | 957+00             | 960+00     | 700  |
| I-15-8(20) 334 SUB-TOTAL 1370      |                    |            |      | I-15-8(20) 334 TOTAL 10,810 10,259 |                    |            |      |
| M.L. 843+00 856+15 1315            |                    |            |      | IG-15-8(18) 334 TOTAL 1583 2855    |                    |            |      |
| R.R. 843+00 853+00 1000            |                    |            |      | I-15-7(58) 332 TOTAL 10,835 14,030 |                    |            |      |
|                                    |                    |            |      | CONTRACT TOTAL 50,372              |                    |            |      |

\* MC 400 @ 0.15 Gal/yd<sup>2</sup>



# SUMMARY SHEET

8  
 1-15-7(58)332  
 1-15-8 20 334  
 16-15-8(18)334  
 Rev 8-30-63  
 Rev 9-16-63

| EARTHWORK SUMMARY |                         |          |                        |                                    |                        |                    |                      |                      |         |
|-------------------|-------------------------|----------|------------------------|------------------------------------|------------------------|--------------------|----------------------|----------------------|---------|
| LINE              | STATION TO STATION      |          | EMBANKMENT<br>CU. YDS. | EMBANKMENT<br>X FACTOR<br>CU. YDS. | EXCAVATION<br>CU. YDS. | BORROW<br>CU. YDS. | OVERHAUL             |                      | REMARKS |
|                   |                         |          |                        |                                    |                        |                    | CLASS A<br>STA. YDS. | CLASS B<br>YD. MILES |         |
|                   | PROJECT NUMBER          |          |                        |                                    |                        |                    |                      |                      |         |
|                   | I-15-7 (58) 332         |          |                        |                                    |                        |                    |                      |                      |         |
| Main              | 700 + 00                | 704 + 18 |                        |                                    | 24,050                 |                    | 240,500              | 1366                 |         |
|                   | 704 + 18                | 711 + 39 |                        |                                    | 37,180                 |                    | 167,310              |                      |         |
|                   | 711 + 39                | 721 + 39 | 18,679                 | 22,415                             | 22,415                 |                    | F.H.                 |                      |         |
|                   | 721 + 39                | 724 + 18 | 30,983                 | 37,180                             |                        |                    |                      |                      |         |
|                   | 724 + 18                | 725 + 93 | 20,042                 | 24,050                             |                        |                    |                      |                      |         |
|                   | 725 + 93                | 727 + 20 | 20,833                 | 25,000                             |                        | 25,000             | 250,000              | 65,530               |         |
|                   | 727 + 20                | 728 + 45 | 21,561                 | 25,873                             |                        | 25,870             | 258,730              | 67,231               |         |
|                   | 728 + 45                | 730 + 40 | 40,939                 | 49,127                             |                        |                    | 491,270              | 198,555              |         |
|                   | 730 + 40                | 732 + 90 | 41,667                 | 50,000                             |                        |                    | 500,000              | 197,822              |         |
|                   | 732 + 90                | 736 + 30 | 41,667                 | 50,000                             |                        |                    | 500,000              | 192,898              |         |
|                   | 736 + 30                | 739 + 30 | 22,492                 | 26,991                             |                        |                    | 269,910              | 101,779              |         |
|                   | 739 + 30                | 739 + 54 | 542                    | 650                                |                        |                    | 6500                 | 870                  |         |
|                   | 739 + 54                | 748 + 19 | 8031                   | 9637                               | 9637                   |                    | F.H.                 |                      |         |
|                   | 748 + 19                | 748 + 67 | 3879                   | 4655                               | 1548                   |                    | 31,070               | 3495                 |         |
|                   | 748 + 67                | 754 + 00 | 3943                   | 4732                               | 4732                   |                    | F.H.                 |                      |         |
|                   | 754 + 00                | 754 + 57 | 327                    | 382                                | 167                    |                    | 2150                 | 212                  |         |
|                   | 754 + 57                | 761 + 00 | 2794                   | 3353                               | 3353                   |                    | F.H.                 |                      |         |
|                   | 761 + 00                | 763 + 89 | 823                    | 988                                | 2169                   |                    | 11,180               | 973                  |         |
|                   | 763 + 89                | 767 + 00 | 1422                   | 1707                               | 1707                   |                    | F.H.                 |                      |         |
|                   | 767 + 00                | 768 + 53 | 1340                   | 1608                               | 660                    |                    | 9480                 | 668                  |         |
|                   | 768 + 53                | 771 + 45 | 6198                   | 7438                               | 355                    |                    | 70,830               | 376                  |         |
|                   | 771 + 45                | 775 + 66 | 11,067                 | 13,281                             | 256                    |                    | 50,798               |                      |         |
|                   | 775 + 66                | 785 + 66 | 16,472                 | 19,767                             | 19,767                 |                    | F.H.                 |                      |         |
|                   | 785 + 66                | 791 + 45 | 8587                   | 10,304                             | 23,329                 |                    |                      |                      |         |
|                   | 791 + 45                | 795 + 00 | 6712                   | 8055                               | 15,138                 |                    |                      |                      |         |
|                   | 795 + 00                | 796 + 87 | 6256                   | 7507                               | 4436                   |                    | 30,710               | 494                  |         |
|                   | 796 + 87                | 808 + 00 | 34,862                 | 41,834                             | 41,834                 |                    | F.H.                 |                      |         |
|                   | 808 + 00                | 812 + 34 | 12,698                 | 15,238                             | 11,563                 |                    | 12,862               |                      |         |
|                   | 812 + 34                | 814 + 00 | 3254                   | 3905                               | 3905                   |                    | F.H.                 |                      |         |
|                   | 814 + 00                | 822 + 95 | 20,217                 | 24,260                             | 24,260                 |                    | F.H.                 |                      |         |
|                   | 822 + 95                | 824 + 34 | 1803                   | 2164                               | 5839                   |                    |                      |                      |         |
|                   | 824 + 34                | 825 + 37 | 975                    | 1170                               | 4241                   |                    |                      |                      |         |
|                   | 825 + 37                | 825 + 70 | 642                    | 771                                | 1719                   |                    |                      |                      |         |
|                   | 825 + 70                | 826 + 20 | 713                    | 856                                | 2037                   |                    |                      |                      |         |
|                   | 826 + 20                | 826 + 35 | 0                      | 0                                  | 215                    |                    |                      |                      |         |
|                   | 826 + 35                | 829 + 20 | 7195                   | 8634                               | 11,741                 |                    |                      |                      |         |
|                   | 829 + 20                | 831 + 00 | 3797                   | 4556                               | 5206                   |                    |                      |                      |         |
|                   | 831 + 00                | 836 + 15 | 21,865                 | 26,238                             | 4459                   |                    | 217,790              | 41,826               |         |
|                   | PROJECT TOTAL           |          | 445,277                | 533,326                            | 287,918                | 50,870             | 3,121,720            | 874,095              |         |
|                   | PROJECT I-15-8 (20) 334 |          |                        |                                    |                        |                    |                      |                      |         |
|                   | 836 + 15                | 836 + 30 | 1024                   | 1230                               |                        |                    | 12,300               | 2309                 |         |
|                   | 836 + 30                | 843 + 00 | 34,347                 | 41,217                             | 442                    |                    | 408,545              | 72,740               |         |
|                   | SUB-TOTAL               |          | 35,371                 | 42,447                             | 442                    |                    | 420,845              | 75,049               |         |
|                   | PROJECT I6-15-8(18) 334 |          |                        |                                    |                        |                    |                      |                      |         |
|                   | 843 + 00                | 844 + 50 | 7688                   | 9225                               |                        |                    | 91,455               | 16,275               |         |
|                   | 844 + 50                | 849 + 30 | 41,667                 | 50,000                             |                        |                    | 500,000              | 79,924               |         |
|                   | 849 + 30                | 851 + 75 | 41,667                 | 50,000                             |                        |                    | 500,000              | 74,148               |         |
|                   | 851 + 75                | 853 + 49 | 36,304                 | 43,565                             |                        |                    | 435,650              | 59,654               |         |
|                   | 853 + 49                | 854 + 90 | 47,029                 | 56,435                             |                        |                    | 564,350              | 48,632               |         |
|                   | 854 + 90                | 856 + 40 | 41,667                 | 50,000                             |                        |                    | 500,000              | 28,127               |         |
|                   | 856 + 40                | 857 + 60 | 41,667                 | 50,000                             |                        |                    | 500,000              | 24,526               |         |
|                   | 857 + 60                | 858 + 60 | 41,667                 | 50,000                             |                        |                    | 500,000              | 22,443               |         |
|                   | 858 + 60                | 859 + 55 | 41,667                 | 50,000                             |                        |                    | 500,000              | 19,981               |         |

| EARTHWORK SUMMARY (CONTINUED) |                         |          |                        |                                    |                        |                    |                      |                      |         |
|-------------------------------|-------------------------|----------|------------------------|------------------------------------|------------------------|--------------------|----------------------|----------------------|---------|
| LINE                          | STATION TO STATION      |          | EMBANKMENT<br>CU. YDS. | EMBANKMENT<br>X FACTOR<br>CU. YDS. | EXCAVATION<br>CU. YDS. | BORROW<br>CU. YDS. | OVERHAUL             |                      | REMARKS |
|                               |                         |          |                        |                                    |                        |                    | CLASS A<br>STA. YDS. | CLASS B<br>YD. MILES |         |
| Main                          | 859 + 55                | 860 + 60 | 41,667                 | 50,000                             |                        |                    | 500,000              | 17,519               |         |
|                               | 860 + 60                | 861 + 55 | 41,667                 | 50,000                             |                        |                    | 500,000              | 15,625               |         |
|                               | 861 + 55                | 862 + 50 | 41,667                 | 50,000                             |                        |                    | 500,000              | 13,920               |         |
|                               | 862 + 50                | 863 + 00 | 16,026                 | 19,231                             |                        |                    | 192,310              | 4,444                |         |
|                               | PROJECT TOTAL           |          | 482,050                | 578,456                            |                        |                    | 5,783,765            | 425,218              |         |
|                               | PROJECT I-15-8 (20) 334 |          |                        |                                    |                        |                    |                      |                      |         |
|                               | 863 + 00                | 863 + 80 | 25,641                 | 30,769                             |                        |                    | 307,690              | 7,109                |         |
|                               | 863 + 80                | 864 + 95 | 41,667                 | 50,000                             |                        |                    | 500,000              | 9375                 |         |
|                               | 864 + 95                | 866 + 55 | 41,667                 | 50,000                             |                        |                    | 500,000              | 6723                 |         |
|                               | 866 + 55                | 869 + 53 | 51,579                 | 61,895                             |                        |                    | 618,950              | 3282                 |         |
|                               | 869 + 53                | 874 + 27 | 73,462                 | 88,155                             |                        |                    | 396,698              |                      |         |
|                               | 874 + 27                | 884 + 27 | 50,945                 | 61,134                             | 61,134                 |                    | F.H.                 |                      |         |
|                               | 884 + 27                | 889 + 53 |                        |                                    | 88,155                 |                    |                      |                      |         |
|                               | 889 + 53                | 892 + 00 |                        |                                    | 61,895                 |                    |                      |                      |         |
|                               | 892 + 00                | 893 + 60 |                        |                                    | 50,000                 |                    |                      |                      |         |
|                               | 893 + 60                | 894 + 90 |                        |                                    | 50,000                 |                    |                      |                      |         |
|                               | 894 + 90                | 896 + 05 |                        |                                    | 50,000                 |                    |                      |                      |         |
|                               | 896 + 05                | 897 + 10 |                        |                                    | 50,000                 |                    |                      |                      |         |
|                               | 897 + 10                | 898 + 10 |                        |                                    | 50,000                 |                    |                      |                      |         |
|                               | 898 + 10                | 899 + 20 |                        |                                    | 50,000                 |                    |                      |                      |         |
|                               | 899 + 20                | 901 + 00 |                        |                                    | 50,000                 |                    |                      |                      |         |
|                               | 901 + 00                | 902 + 40 |                        |                                    | 50,000                 |                    |                      |                      |         |
|                               | 902 + 40                | 903 + 65 |                        |                                    | 50,000                 |                    |                      |                      |         |
|                               | 903 + 65                | 909 + 25 |                        |                                    | 50,000                 |                    |                      |                      |         |
|                               | 909 + 25                | 927 + 00 | 452                    | 542                                | 56,977                 |                    |                      |                      |         |
|                               | 927 + 00                | 930 + 37 | 12,238                 | 14,685                             |                        |                    | 36,712               |                      |         |
|                               | 930 + 37                | 940 + 37 | 10,672                 | 12,806                             | 12,806                 |                    | F.H.                 |                      |         |
|                               | 940 + 37                | 942 + 76 |                        |                                    | 14,685                 |                    |                      |                      |         |
|                               | 942 + 76                | 947 + 00 |                        |                                    | 43,565                 |                    |                      |                      |         |
|                               | 947 + 00                | 950 + 20 |                        |                                    | 50,000                 |                    |                      |                      |         |
|                               | 950 + 20                | 953 + 00 |                        |                                    | 50,000                 |                    |                      |                      |         |
|                               | 953 + 00                | 955 + 25 |                        |                                    | 50,000                 |                    |                      |                      |         |
|                               | 955 + 25                | 955 + 30 |                        |                                    | 1230                   |                    |                      |                      |         |
|                               | 955 + 30                | 956 + 15 |                        |                                    | 21,779                 |                    |                      |                      |         |
|                               | 956 + 15                | 957 + 30 |                        |                                    | 26,991                 |                    |                      |                      |         |
|                               | 957 + 30                | 959 + 50 |                        |                                    | 50,000                 |                    |                      |                      |         |
|                               | 959 + 50                | 961 + 60 |                        |                                    | 50,000                 |                    |                      |                      |         |
|                               | 961 + 60                | 964 + 00 |                        |                                    | 49,127                 |                    |                      |                      |         |
|                               | SUB-TOTAL               |          | 308,323                | 369,986                            | 1,188,344              |                    | 2,369,050            | 26,489               |         |
|                               | I-15-8 (20) 334 TOTAL   |          | 343,694                | 412,433                            | 1,188,786              |                    | 2,780,895            | 101,538              |         |
|                               | I6-15-8 (18) 334 TOTAL  |          | 482,050                | 578,456                            | 0                      |                    | 5,783,765            | 425,218              |         |
|                               | I-15-7 (58) 332 TOTAL   |          | 445,277                | 533,326                            | 287,918                | 50,870             | 3,121,720            | 874,095              |         |
|                               | CONTRACT TOTAL          |          | 1,271,021              | 1,524,215                          | 1,476,704              | 50,870             | 11,686,380           | 1,400,851            |         |
|                               | USE                     |          | 1,335,000              | 1,600,000                          | 1,551,000              | 53,400             | 12,271,000           | 1,471,000            |         |



1-15-7 (58) 332  
1-15-8 (20) 334  
16-15-8 (18) 334

Revised Aug. 30, 1963  
Revised Oct. 7, 1963

# SUMMARY SHEET

| WATERING, ROLLING & COMPACTION   |                      | WATERING, ROLLING & COMPACTION (CONT.)  |            |
|--|----------------------|---|------------|
| DESCRIPTION  | USE                  | DESCRIPTION   | USE        |
| <b>WATERING</b>  |                      | <b>ROLLING, PNEUMATIC TIRED OR POWER ROLLER</b>   |            |
| <i>All Embankment</i>  |                      | <i>Riverdale Road Acceleration Lane</i>   |            |
| I-15-7 (58) 332 533, 326 x 40 = 21, 333, 040 Gals.   | 22, 400 M-Gals.      | Portland Cement Treated Base Course   |            |
| IG-15-8 (18) 334 578, 456 x 40 = 23, 138, 240 Gals.  | 24, 300 M-Gals.      | (44.17 + 45.5) x 600 ÷ 9 ft. <sup>2</sup> /Yd. <sup>2</sup> ÷ 1800 Yd. <sup>2</sup> /Hr. = 3 Hrs.     |            |
| I-15-8 (20) 334 412, 433 x 40 = 16, 497, 320 Gals.   | 17, 300 M-Gals.      | Gravel Base Course  |            |
|  |                      | (46.83' + 47.83') x 600 ÷ 9 ft. <sup>2</sup> /Yd. <sup>2</sup> ÷ 1800 Yd. <sup>2</sup> /Hr. = 3 Hrs.  |            |
| <i>All Gravel Material</i>   |                      | TOTAL 6 Hrs.  | 10 Hrs.    |
| 24, 337 Tons x 0.53 x 40 = 51, 594 Gals.   | 600 M-Gals.          | "F-5" Acceleration Lane   |            |
| GRAND TOTAL  | 64, 600 M-Gals.      |   |            |
| <b>COMPACTION</b>  |                      | <b>MECHANICAL TAMPING</b>   |            |
| I-15-7 (58) 332  |                      | (25.67' + 27') x 875' ÷ 9 ft. <sup>2</sup> /Yd. <sup>2</sup> ÷ 1800 Yd. <sup>2</sup> /Hr. = 3 Hrs.    |            |
| Total Embankment 533, 326 cu. Yds.   | 587, 000 cu. Yds.    | (28.33' + 29.33') x 875' ÷ 9 ft. <sup>2</sup> /Yd. <sup>2</sup> ÷ 1800 Yd. <sup>2</sup> /Hr. = 3 Hrs. |            |
| I-15-8 (20) 334 412, 433 cu. Yds.  | 454, 000 cu. Yds.    | TOTAL 6 Hrs.  | 10 Hrs.    |
| IG-15-8 (18) 334 578, 456 cu. Yds.   | 636, 000 cu. Yds.    | I-15-8 (20) 334 Project Total   | 740 Hrs.   |
| GRAND TOTAL  | 1, 677, 000 cu. Yds. | GRAND TOTAL   | 1 070 Hrs. |
| <b>ROLLING PNEUMATIC TIRED OR POWER ROLLER</b>   |                      | <b>MECHANICAL TAMPING</b>   |            |
| "V" Line   |                      | I-15-7 (58) 332 700 cu. Yd. ÷ 8 cu. Yd./Hr. = 87.5 Hrs.   | 88 Hrs.    |
|  |                      | I-15-8 (20) 334 350 cu. Yd. ÷ 8 cu. Yd./Hr. = 43.8 Hrs.   | 44 Hrs.    |
|  |                      | IG-15-8 (18) 334 230 cu. Yd. ÷ 8 cu. Yd./Hr. = 28.6 Hrs.  | 29 Hrs.    |
|  |                      | GRAND TOTAL   | 161 Hrs.   |
| <i>Gravel Surface Type "A"</i>   |                      |   |            |
| 76' x 2 x 2192' ÷ 9 ft. <sup>2</sup> /Yd. <sup>2</sup> ÷ 1800 Yd. <sup>2</sup> /Hr. = 206 Hrs.       |                      |   |            |
| <i>Gravel Base Course</i>  |                      |   |            |
| 79' x 2192' ÷ 9 ft. <sup>2</sup> /Yd. <sup>2</sup> ÷ 1800 Yd. <sup>2</sup> /Hr. = 107 Hrs.           |                      |   |            |
| I-15-7 (58) 332 Project Total = 313 Hrs.   | 330 Hrs.             |   |            |
| 5600 South Street  |                      |   |            |
| <i>Gravel Surface Type "A" &amp; Gravel Base Course</i>  |                      |   |            |
| 4' x 44' x 900' ÷ 9 ft. <sup>2</sup> /Yd. <sup>2</sup> ÷ 1800 Yd. <sup>2</sup> /Hr. = 98 Hrs.        |                      |   |            |
| TOTAL 98 Hrs.  | 110 Hrs.             |   |            |
| <i>"R-7", "R-8", "R-9" &amp; "R-10" Lines</i>  |                      |   |            |
| <i>Gravel Surface Type "A"</i>   |                      |   |            |
| 32' x 2 x 8632.22' ÷ 9 ft. <sup>2</sup> /Yd. <sup>2</sup> ÷ 1800 Yd. <sup>2</sup> /Hr. = 341 Hrs.    |                      |   |            |
| <i>Gravel Base Course</i>  |                      |   |            |
| 35' x 8632.22' ÷ 9 ft. <sup>2</sup> /Yd. <sup>2</sup> ÷ 1800 Yd. <sup>2</sup> /Hr. = 186 Hrs.        |                      |   |            |
| TOTAL 527 Hrs.   | 560 Hrs.             |   |            |
| <i>"F-5" Line Sta. 10+00 to 17+88.16 16' Median</i>  |                      |   |            |
| <i>Sta. 17+88.16 to 57+08.35 0' Median</i>   |                      |   |            |
| <i>Gravel Surface Type "A"</i>   |                      |   |            |
| (46' + 48') x 788.16' ÷ 9 ft. <sup>2</sup> /Yd. <sup>2</sup> ÷ 1800 Yd. <sup>2</sup> /Hr. = 5 Hrs.   |                      |   |            |
| (30' + 32') x 3920.19' ÷ 9 ft. <sup>2</sup> /Yd. <sup>2</sup> ÷ 1800 Yd. <sup>2</sup> /Hr. = 15 Hrs. |                      |   |            |
| <i>Gravel Base Course</i>  |                      |   |            |
| 50' x 788.16' ÷ 9 ft. <sup>2</sup> /Yd. <sup>2</sup> ÷ 1800 Yd. <sup>2</sup> /Hr. = 2 Hrs.           |                      |   |            |
| 34' x 3920.19' ÷ 9 ft. <sup>2</sup> ÷ 1800 Yd. <sup>2</sup> /Hr. = 8 Hrs.                            |                      |   |            |
| TOTAL 30 Hrs.  | 40 Hrs.              |   |            |
| <i>"F-5A" Line</i>   |                      |   |            |
| <i>Gravel Surface &amp; Gravel Base</i>  |                      |   |            |
| (18' + 20' + 22) x 287 ÷ 9 ft. <sup>2</sup> /Yd. <sup>2</sup> ÷ 1800 Yd. <sup>2</sup> /Hr. 1 Hr.     |                      |   |            |
| TOTAL 1 Hrs.   | 10 Hrs.              |   |            |



1-15-7 (58) 332  
1-15-8 (20) 334  
16-15-8 (18) 334

Revised Aug. 30, 1963  
Revised Sept. 16, 1963  
Revised Oct. 7, 1963

# SUMMARY SHEET

## MEDIAN CURB TYPE "E"

| LINE          | STATION |       | LINEAR FEET |
|---------------|---------|-------|-------------|
|               | FROM    | TO    |             |
| "V"           | 10+60   | 14+43 | 667         |
| "V"           | 15+27   | 19+18 | 815         |
| "V"           | 19+75   | 21+69 | 430         |
| TOTAL         |         |       | 1912        |
| USE           |         |       | 1910        |
| 5600 SO.      | 4+20    | 6+15  | 397         |
| 5600 SO.      | 6+74    | 11+33 | 940         |
| 5600 SO.      | 12+52   | 13+20 | 140         |
| "F-5"         | 10+60   | 17+02 | 1380        |
| TOTAL         |         |       | 2857        |
| USE           |         |       | 2860        |
| PROJECT TOTAL |         |       | 4770        |

## SUMMARY OF ITEMS

| DESCRIPTION<br>PROJECT                                    | UNIT     | QUANTITY       |                |                 | TOTAL      |
|---|----------|----------------|----------------|-----------------|------------|
|   |          | I-15-7(58) 332 | I-15-8(20) 334 | IG-15-8(18) 334 |            |
| Furnishing Construction Signs                             | Lump     | Lump           | Lump           | Lump            | Lump       |
| Removal of Trees  | Each     | 0              | 4127           | 1530            | 30         |
| Flagging  | Man Hr.  | 600            | 1000           | 1000            | 1600       |
| Unclassified Roadway Excavation                           | Cu. Yd.  | 303,000        | 1,248,000      | 0               | 1,551,000  |
| Excavation for Structures Unclassified                    | Cu. Yd.  | 1100           | 450            | 100             | 1650       |
| Demolition of Buildings, Basements, Foundations and Slabs | Lump     | Lump           | Lump           | Lump            | Lump       |
| Mechanical Tamping  | Hour     | 88             | 44             | 29              | 161        |
| Imported Borrow   | Cu. Yd.  | 53,400         |                |                 | 53,400     |
| Compaction Method "B"                                     | Cu. Yd.  | 560,000        | 433,000        | 607,000         | 1,600,000  |
| Rolling Pneumatic Tired or Power Roller                   | Hour     | 330            | 740            | 0               | 1070       |
| Class "A" Overhaul  | Sq. Yd.  | 3,278,000      | 2,920,000      | 6,073,000       | 12,271,000 |
| Class "B" Overhaul  | Yd. Mile | 918,000        | 107,000        | 446,000         | 1,471,000  |
| Furnishing Water Equipment                                | Lump     | Lump           | Lump           | Lump            | Lump       |
| Watering  | M-Gal.   | 22,400         | 17,900         | 24,300          | 64,600     |
| Right of Way Fence 6' Chain Link                          | Lin. Ft. | 24,865         | 21,069         | 4438            | 50,372     |
| 12' Gates Chain Link                                      | Each     | 2              |                |                 | 2          |
| 12" Reinforced Concrete Pipe                              | Lin. Ft. |                | 300            |                 | 300        |
| 24" Reinforced Concrete Pipe                              | Lin. Ft. | 1434           | 5560           | 508             | 7502       |
| 30" Reinforced Concrete Pipe                              | Lin. Ft. | 198            |                |                 | 198        |
| 24" Reinforced Concrete End Sections                      | Each     | 22             | 21             | 4               | 47         |
| 30" Reinforced Concrete End Sections                      | Each     | 4              |                |                 | 4          |
| Gravel Base Course  | Ton      | 1600           |                | 9600            | 11,200     |
| Portland Cement Treated Base Course                       | Ton      |                | 4000           |                 | 4000       |
| Portland Cement Type I                                    | Bag      |                | 1700           |                 | 1700       |
| Bituminous Material MC-400                                | Ton      | 20             | 90             |                 | 110        |
| Bituminous Material MC-800                                | Ton      | 10             | 50             |                 | 60         |
| Bituminous Material MC-70                                 | Ton      | 10             | 40             |                 | 50         |
| Gravel Surface Type "A"                                   | Ton      | 2660           | 14,400         |                 | 17,060     |
| Bituminous Material 60-70 Penetration                     | Ton      | 90             | 480            |                 | 570        |
| Bituminous Surface Course 1" Max.                         | Ton      | 1590           | 9265           |                 | 10,855     |
| Cover Material Type "A"                                   | Ton      | 110            | 530            |                 | 640        |
| Surface Ditches   | Lin. Ft. |                | 1700           | 1900            | 3600       |
| Small Ditch Excavation                                    | Cu. Yd.  |                | 130            | 100             | 230        |
| Concrete Curb "E"   | Lin. Ft. | 1910           | 2860           |                 | 4770       |
| Deep Beam Highway Guard Rail                              | Lin. Ft. | 187            | 249            |                 | 436        |
| Metal Bin Type "A" Wall                                   | Sq. Ft.  |                |                | 1000            | 1000       |
| Metal Bin Type "B" Wall                                   | Sq. Ft.  |                |                | 762             | 762        |
| Right of Way Markers                                      | Each     | 30             | 30             | 4               | 64         |
| Obliteration of Old Road                                  | Mile     | 0.5            |                |                 | 0.5        |
| Drop Inlet Type "C"                                       | Each     | 2              | 9              | 3               | 14         |
| Excavation and Stockpiling Topsoil                        | Cu. Yd.  | 0              | 20,000         | 0               | 20,000     |
| Concrete Class "A" Air Entaining                          | Cu. Yd.  |                | 30             |                 | 30         |
| Reinforcing Steel   | Lbs.     |                | 3300           |                 | 3300       |
| Structural Steel  | Lbs.     |                | 4800           |                 | 4800       |
| Hydrated Lime   | Ton      | 16             | 90             |                 | 106        |

## MISCELLANEOUS

| DESCRIPTION OF ITEM  | I-15-7(58) 332 | I-15-8(20) 334 | IG-15-8(18) 334 | TOTAL       |
|--|----------------|----------------|-----------------|-------------|
| Furnishing Construction Signs                              | Lump           | Lump           | Lump            | Lump        |
| Flagging   | 600 Mn Hr.     | 1000 Mn Hr.    | 0               | 1600 Mn Hr. |
| Furnishing Water Equipment                                 | Lump           | Lump           | Lump            | Lump        |
| 12' Gates Chain Link                                       | 2 EA           | 0              |                 | 2 EA        |
| Metal Bin Type Retaining Wall Type "A"                     | 0              | 0              | 1000 SF         | 1000 SF     |
| Metal Bin Type Retaining Wall Type "B"                     | 0              | 0              | 762 SF          | 762 SF      |
| Right of Way Markers                                       | 30 EA          | 30 EA          | 4 EA            | 64 EA       |
| Obliteration of Old Road                                   | 0.5 Mi.        | 0              | 0               | 0.5 Mi.     |
| Excavation and Stockpiling Topsoil                         | 0              | 20,000 cY      | 0               | 20,000 cY   |
| Demolition of Buildings, Basements, Foundations and Slabs. | Lump           | Lump           | 0               | Lump        |
| Removal of Trees   | 0              | 1530 EA        | 30 EA           | 1560 EA     |



# SUMMARY SHEET

1-15-7(58)332  
1-15-8(20)334  
1-15-8(18)334  
Rev. Aug. 30, 1963

## DRAINAGE

| LINE   | STATION          | DESCRIPTION                     | MEDIAN DROP INLET REQ'D EACH | R.C.P. REQ'D. LIN. FT. |     |     | R.C.P. END SEC REQ'D. LF. |     |     | SURFACE DITCH REQ'D. - LIN. FT. |        |     |      |        | STRUCT. EXC. CU. YDS. | MECH. TAMPING CU. YD. | SMALL DITCH EXC. CU. YD. | CLASS "A" CONCRETE CU. YD. | REIN. STEEL LBS. | STRUCT. STEEL LBS. | STANDARD DRAWING AND LINE NUMBER | REMARKS   |
|--------|------------------|---------------------------------|------------------------------|------------------------|-----|-----|---------------------------|-----|-----|---------------------------------|--------|-----|------|--------|-----------------------|-----------------------|--------------------------|----------------------------|------------------|--------------------|----------------------------------|---|
|        |                  |                                 |                              | 24"                    | 30" | 48" | 24"                       | 30" | 12" | STA.                            | STA.   | LT. | RT.  | LENGTH |                       |                       |                          |                            |                  |                    |                                  |   |
| Main   | 721 + 80         | Drop Inlet - Cross Drainage     | 1                            | 250                    |     |     | 2                         |     |     |                                 |        |     |      | 93     | 55                    |                       |                          |                            |                  | 346                | C                                |   |
| "U"    | 17 + 35          | Surface Drainage                |                              | 78                     |     |     | 2                         |     |     |                                 |        |     |      |        | 4                     |                       |                          |                            |                  |                    |                                  |   |
| "T"    | 15 + 85          | Surface Drainage                |                              | 68                     |     |     | 2                         |     |     |                                 |        |     |      |        | 4                     |                       |                          |                            |                  |                    |                                  |   |
| Main   | 763 + 65         | Drop Inlet                      | 1                            | 300                    |     |     |                           |     |     |                                 |        |     |      | 82     | 96                    |                       |                          |                            |                  | 346                | C                                | Drop Inlets Req'd on E. Connect Into New 24" Storm Sewer.   |
|        | 784 + 00         | Cross Drainage                  |                              | 192                    |     |     | 4                         |     |     |                                 |        |     |      | 185    | 80                    |                       |                          |                            |                  |                    |                                  |   |
|        | 795 + 00         | Cross Drainage                  |                              |                        | 198 |     | 4                         |     |     |                                 |        |     |      | 180    | 185                   |                       |                          |                            |                  |                    |                                  |   |
|        | 806 + 00         | Cross Drainage                  |                              | 174                    |     |     | 4                         |     |     |                                 |        |     |      | 142    | 72                    |                       |                          |                            |                  |                    |                                  |   |
|        | 817 + 00         | Cross Drainage                  |                              | 178                    |     |     | 4                         |     |     |                                 |        |     |      | 145    | 72                    |                       |                          |                            |                  |                    |                                  |   |
|        | 827 + 00         | Cross Drainage                  |                              | 194                    |     |     | 4                         |     |     |                                 |        |     |      | 187    | 80                    |                       |                          |                            |                  |                    |                                  |   |
|        | TOTAL            | I-15-7 (58) 332                 | 2                            | 1434                   | 198 |     | 22                        | 4   |     |                                 |        |     |      | 1014   | 648                   |                       |                          |                            |                  |                    |                                  |   |
|        | USE              |                                 | 2                            | 1434                   | 198 |     | 22                        | 4   |     |                                 |        |     |      | 1100   | 700                   |                       |                          |                            |                  |                    |                                  |   |
| Main   | 849 + 00         | Drop Inlet - Cross Drainage     | 1                            | 234                    |     |     | 2                         |     |     | 849+00                          | 859+00 | Lt. | 100' | 75     | 96                    |                       |                          |                            |                  | 346                | C                                | Drop Inlet Req'd in Median                                  |
|        | 860 + 00         | Cross Drainage                  | 1                            | 274                    |     |     | 2                         |     |     | 860+00                          | 869+00 | Lt. | 90'  | 112    | 100                   |                       |                          |                            |                  | 346                | C                                | Drop Inlet Req'd in Median                                  |
|        | TOTAL            | IG-15-8 (18) 334                | 3                            | 508                    |     |     | 4                         |     |     |                                 |        |     |      | 1900   | 75                    | 208                   | 100                      |                            |                  |                    |                                  |   |
|        | USE              |                                 | 3                            | 508                    |     |     | 4                         |     |     |                                 |        |     |      | 1900   | 100                   | 230                   | 100                      |                            |                  |                    |                                  |   |
|        | 839 + 50         | Drop Inlet - Cross Drainage     | 1                            | 192                    |     |     | 2                         |     |     | 836+15                          | 839+50 | Lt. | 335' |        |                       |                       |                          |                            |                  | 346                | C                                | Drop Inlet Req'd in Median                                  |
|        |                  |                                 |                              |                        |     |     |                           |     |     | 871+00                          | 879+00 | Lt. | 800' |        |                       |                       |                          |                            |                  |                    |                                  |   |
|        | 880 + 00         | Median Drain S.B. Lane          | 1                            | 124                    |     |     | 1                         |     |     |                                 |        |     |      |        | 52                    |                       |                          |                            |                  | 346                | C                                | Drop Inlet Req'd in Median                                  |
|        | 890 + 00         | Drop Inlet - Cross Drainage     | 1                            | 156                    |     |     | 2                         |     |     |                                 |        |     |      | 72     | 64                    |                       |                          |                            |                  | 346                | C                                | Drop Inlet Req'd in Median                                  |
| "F-3"  | 13 + 85          | Cross Drainage                  |                              | 124                    |     |     |                           |     |     |                                 |        |     |      |        | 51                    |                       |                          |                            |                  |                    |                                  |   |
|        |                  |                                 |                              |                        |     |     |                           |     |     | 10+60                           | 15+50  | Lt. | 490' |        |                       |                       |                          |                            |                  |                    |                                  |   |
| "F-5"  | 29 + 50          | Cross Drainage at 890+00 ± I-15 |                              | 76                     |     |     | 2                         |     |     |                                 |        |     |      | 56     | 31                    |                       |                          |                            |                  |                    |                                  |   |
| Main   | 900 + 00         | Ditch & Median Drainage         | 1                            | 178                    |     |     | 2                         |     |     |                                 |        |     |      | 17     | 10                    |                       | 3.4                      | 420                        | 620              | V-753<br>346       | C                                | C.B. 81' Lt. Line 2 C.B. 81' Rt. Line 1                     |
|        | 900+00 to 909+50 | Storm Sewer Along Median E.     |                              | 950                    |     |     |                           |     |     |                                 |        |     |      |        |                       |                       |                          |                            |                  |                    |                                  |   |
|        | 909 + 50         | Drop Inlet Cross Drainage       | 1                            | 750                    |     |     | 2                         |     |     |                                 |        |     |      | 17     | 10                    |                       | 3.4                      | 420                        | 620              | V-753<br>346       | C                                | C.B. 81' Lt. Line 2 C.B. 81' Rt. Line 1                     |
|        | 919 + 00         | Cut Ditch & Median Drainage     | 1                            | 144                    |     |     | 2                         |     |     |                                 |        |     |      | 18     | 10                    |                       | 3.8                      | 457                        | 620              | V-753<br>346       | C                                | C.B. 81' Lt. Line 3 C.B. 81' Rt. Line 2                     |
|        | 919+00 to 927+50 | Storm Sewer Along Median E.     |                              | 850                    |     |     |                           |     |     |                                 |        |     |      |        |                       |                       |                          |                            |                  |                    |                                  |   |
|        | 927 + 50         | Drop Inlet - Cross Drainage     | 1                            | 144                    |     |     | 2                         |     |     |                                 |        |     |      | 18     | 10                    |                       | 3.6                      | 432                        | 620              | V-753<br>346       | C                                | C.B. 81' Lt. Line 2 C.B. 81' Rt. Line 2                     |
|        | 938 + 00         | Drop Inlet - Cross Drainage     | 1                            | 144                    |     |     | 2                         |     |     |                                 |        |     |      | 17     | 10                    | 18                    | 3.4                      | 420                        | 620              | V-753<br>346       | C                                | C.B. 81' Lt. Line 2 C.B. 81' Rt. Line 1                     |
|        | 950 + 00         | Cut Ditch & Median Drainage     | 1                            | 144                    |     |     | 2                         |     |     |                                 |        |     |      | 17     | 10                    |                       | 3.4                      | 420                        | 620              | V-753<br>346       | C                                | C.B. 81' Lt. Line 2 C.B. 81' Rt. Line 1                     |
|        | 950+00 to 963+00 | Storm Sewer Along Median E.     |                              | 1300                   |     |     |                           |     |     |                                 |        |     |      |        |                       |                       |                          |                            |                  |                    |                                  |   |
|        | 963 + 00         | Drop Inlet - Cross Drainage     | 1                            | 144                    |     |     | 2                         |     |     |                                 |        |     |      | 17     | 10                    | 12                    | 3.4                      | 420                        | 620              | V-753<br>346       | C                                | C.B. 81' Lt. Line 2 C.B. 81' Rt. Line 1 Outlet Ditch Req'd. |
| "R-10" | 14 + 20          | Cross Drainage at 919+00        |                              | 40                     |     |     | 2                         |     |     |                                 |        |     |      |        |                       |                       |                          |                            |                  |                    |                                  |   |
| Main   | 881+00 to 908+00 | Pave Median & Cut Ditches       |                              |                        |     |     |                           |     |     |                                 |        |     |      |        |                       |                       |                          |                            |                  |                    |                                  |   |
| Main   | 932+00 to 964+00 | Pave Median & Cut Ditches       |                              |                        |     |     |                           |     |     |                                 |        |     |      |        |                       |                       |                          |                            |                  |                    |                                  |   |
| "R-10" | 10 + 30          | Irrigation                      |                              | 100                    |     |     |                           |     |     |                                 |        |     |      | 52     | 34                    |                       |                          |                            |                  |                    |                                  |   |
| Main   | 912+40 to 915+00 | Irrigation                      |                              |                        |     |     |                           |     |     |                                 |        |     |      |        |                       | 93                    |                          |                            |                  |                    |                                  | Small Ditch Req'd. Outside N/A Line                         |
| Main   | 945 to 948 Rt.   | Irrigation                      |                              |                        |     |     |                           |     |     |                                 |        |     |      | 83     |                       |                       |                          |                            |                  |                    |                                  |   |
|        | TOTAL            | I-15-8 (20) 334                 | 9                            | 5560                   | 198 |     | 21                        | 300 |     |                                 |        |     |      | 1625   | 384                   | 302                   | 123                      | 24.4                       | 2989             | 4340               |                                  |   |
|        | USE              |                                 | 9                            | 5560                   | 198 |     | 21                        | 300 |     |                                 |        |     |      | 1700   | 450                   | 350                   | 130                      | 30                         | 3300             | 4800               |                                  |   |
|        | GRAND TOTAL      |                                 | 14                           | 7502                   | 198 |     | 47                        | 4   | 300 |                                 |        |     |      | 3600   | 1650                  | 1280                  | 230                      | 30                         | 3300             | 4800               |                                  |   |











| FED. ROAD DIST. NO. | STATE | PROJ. NO.     | FISCAL YEAR | SHEET NO.   | TOTAL SHEETS |
|---------------------|-------|---------------|-------------|-------------|--------------|
| 15                  | UTAH  | 1-15-7(58)334 | 196         | 6           |              |
|                     |       | 15 818 334    |             |             |              |
|                     |       | Rev 7-12-66 B | Rev 8-30-63 | Rev 1-27-64 |              |
|                     |       |               | Rev 9-16-63 | Rev 3-24-64 |              |

HILL FIELD INTERCHANGE

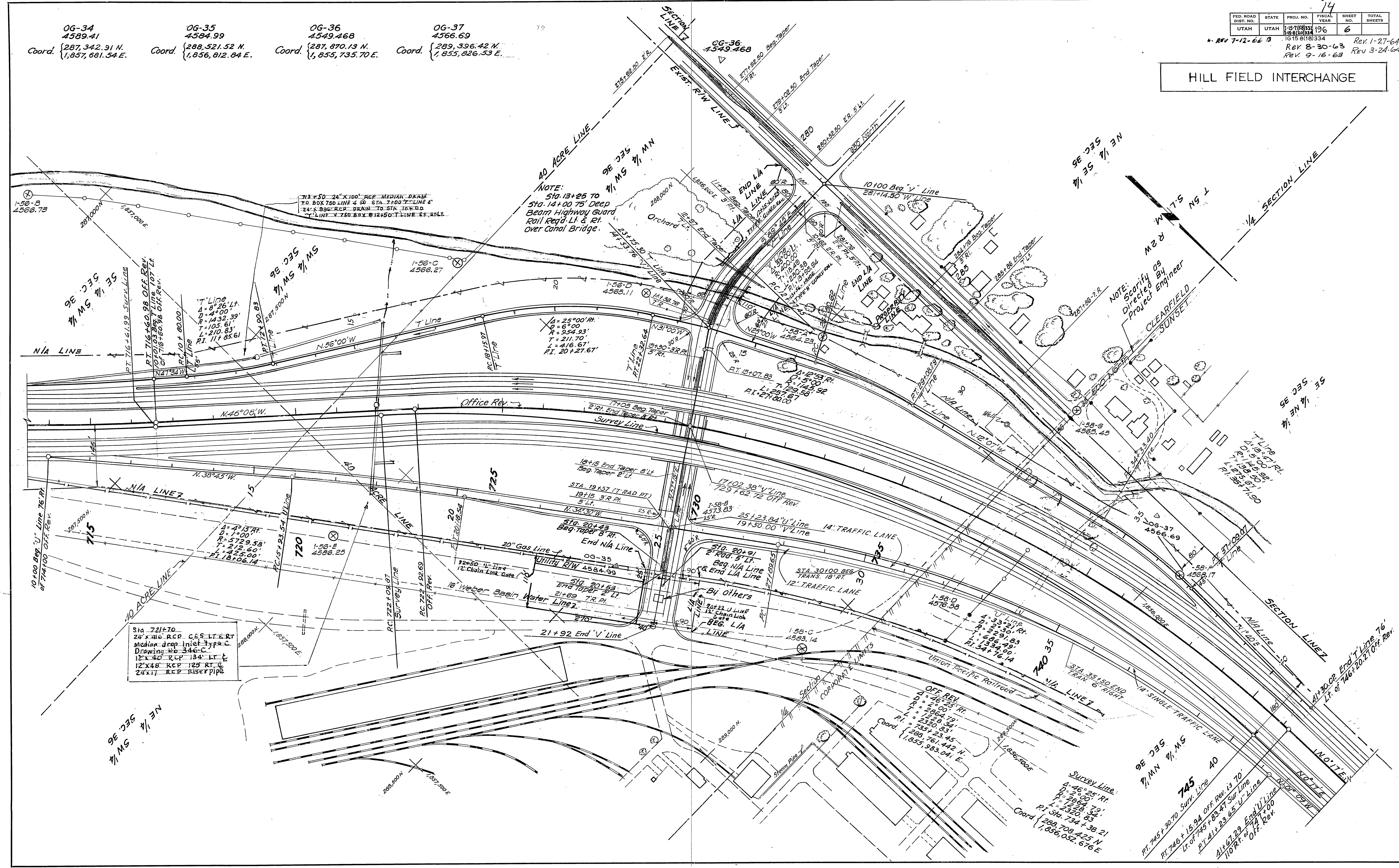
06-34  
4589.41  
Coord. {287,342.91 N.  
1,857,681.54 E.

06-35  
4584.99  
Coord. {288,521.52 N.  
1,856,812.84 E.

06-36  
4549.468  
Coord. {287,870.13 N.  
1,855,735.70 E.

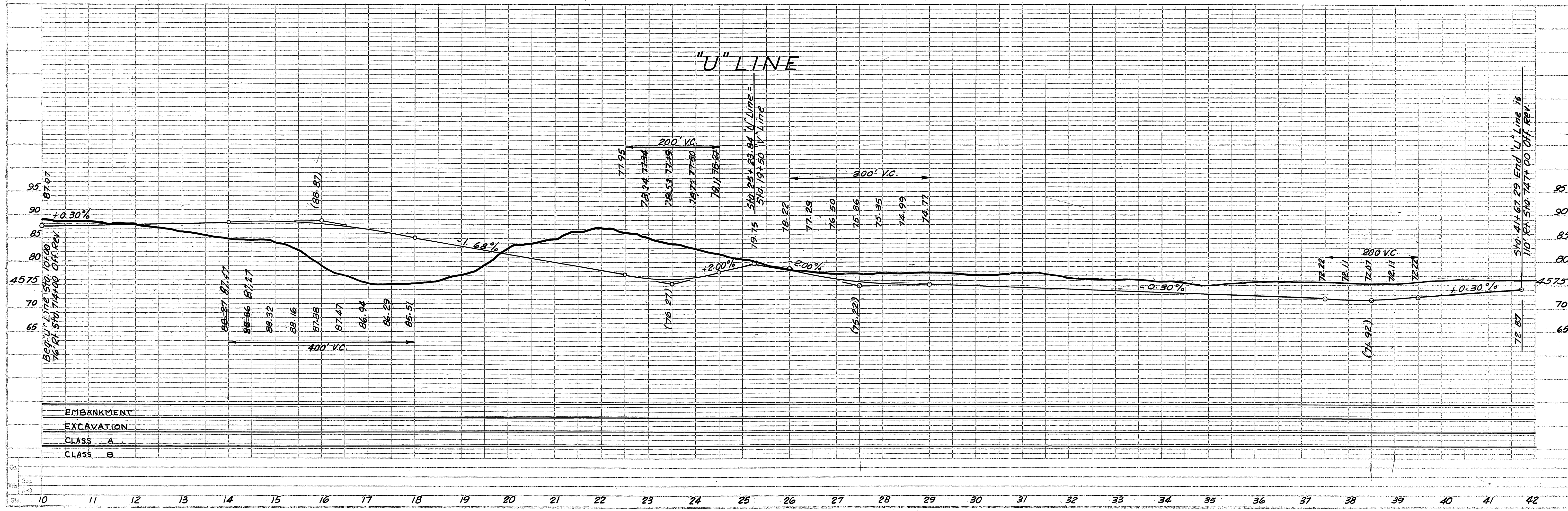
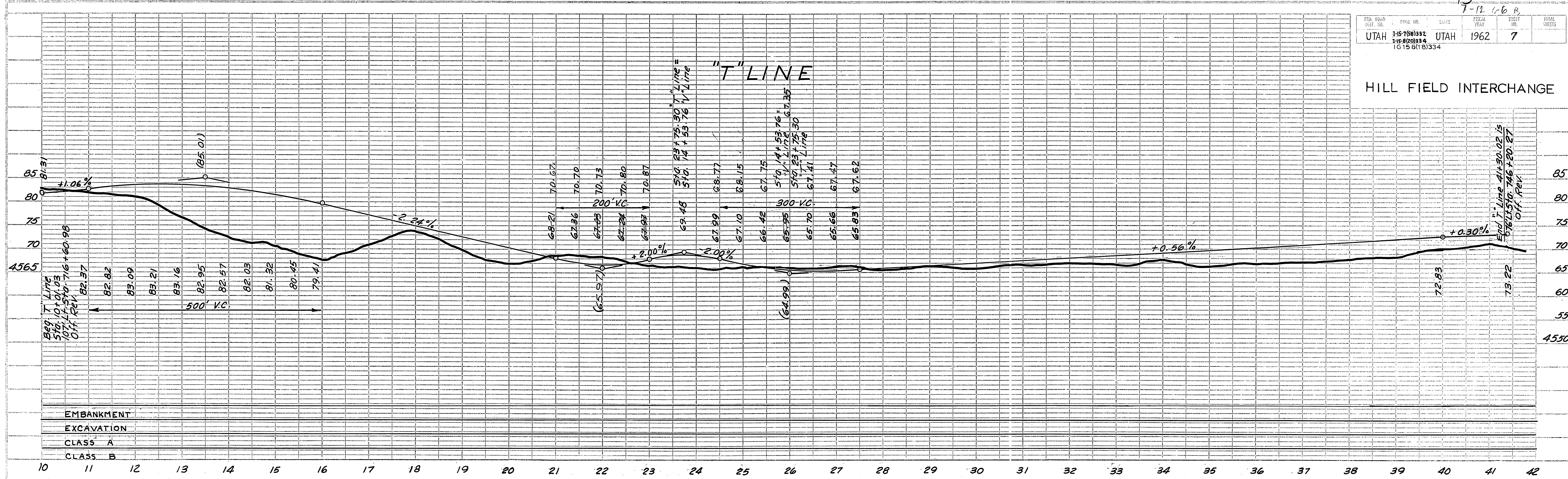
06-37  
4566.69  
Coord. {289,396.42 N.  
1,855,826.53 E.

| REVISIONS | DATE | BY           |
|-----------|------|--------------|
| 1         |      | Y.F. Stevens |



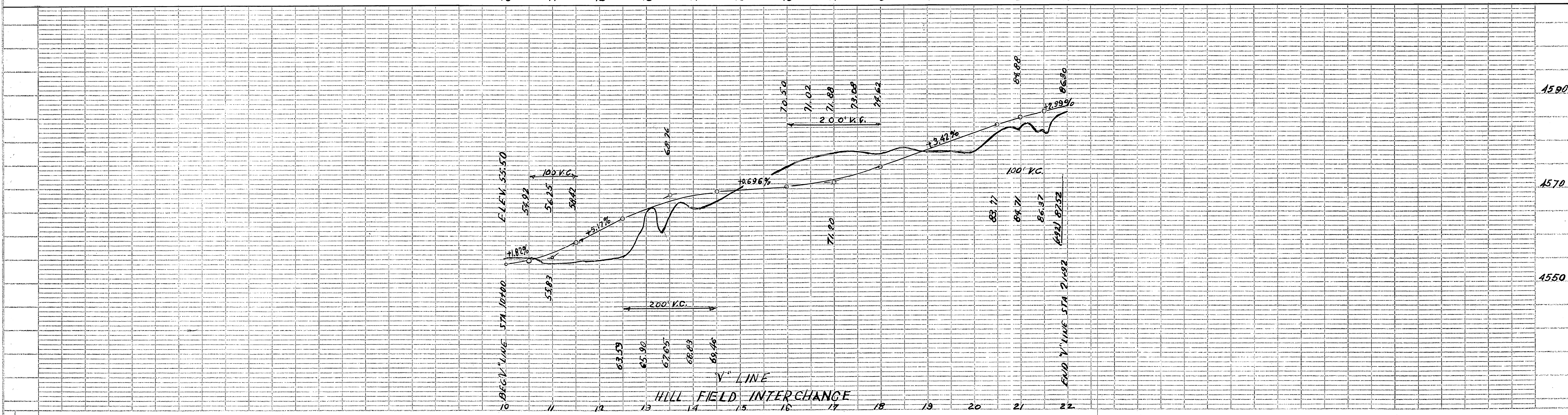
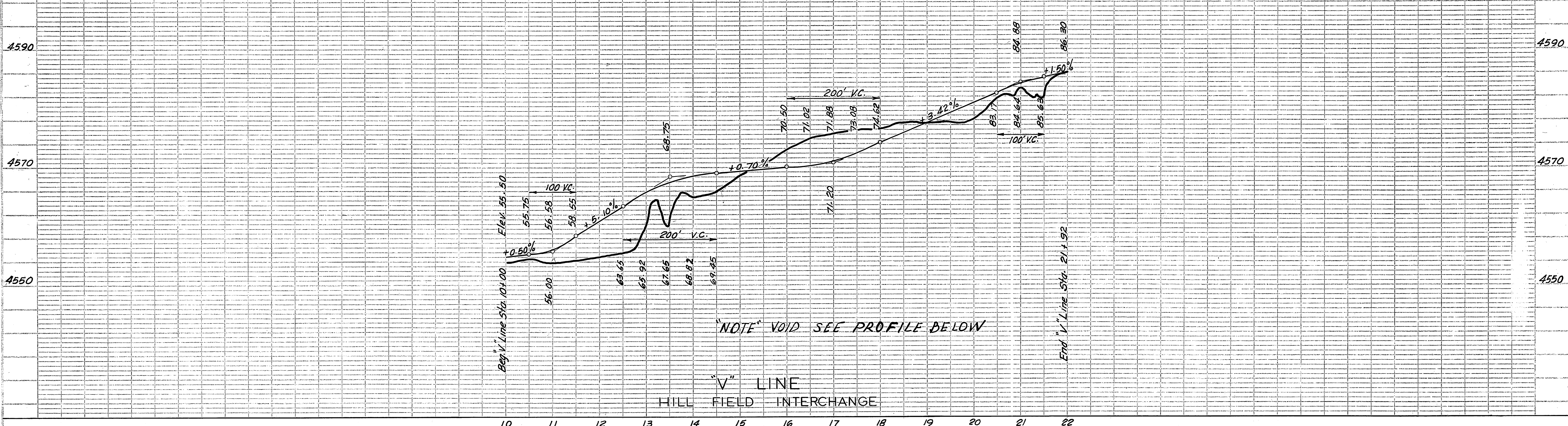


HILL FIELD INTERCHANGE





|                      |               |       |             |           |              |
|----------------------|---------------|-------|-------------|-----------|--------------|
| FED. ROAD DIST. NO.  | PROJ. NO.     | STATE | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|                      | 1-15-76(8)334 |       |             | 8         |              |
| A.C. REV. 7-18-66 B. |               |       |             |           |              |



10 11 12 13 14 15 16 17 18 19 20 21 22

10 11 12 13 14 15 16 17 18 19 20 21 22

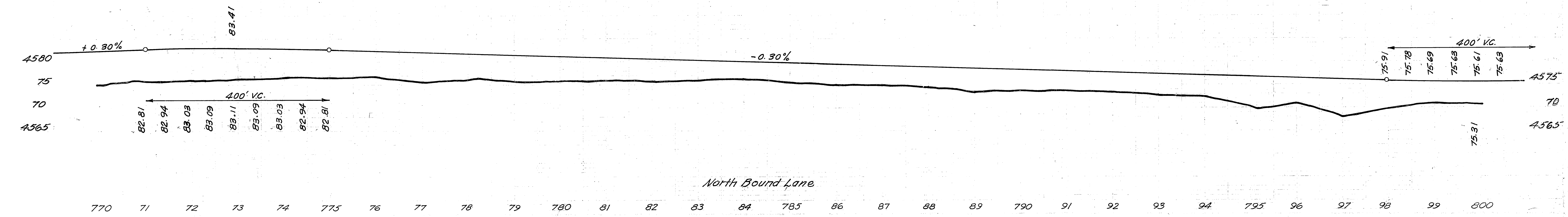
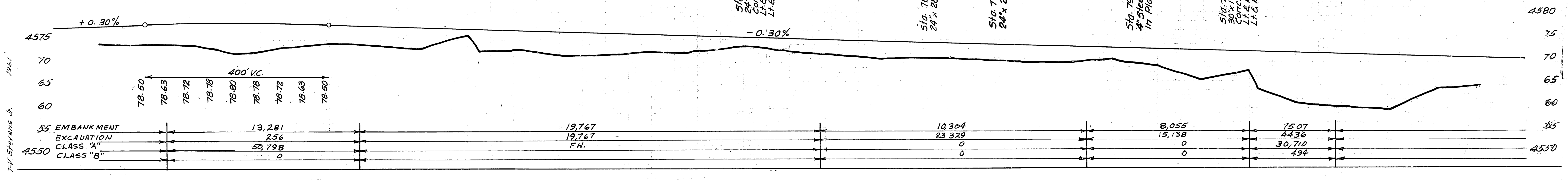
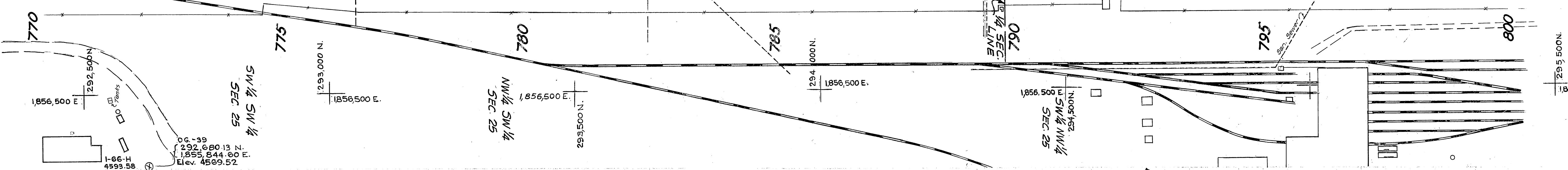
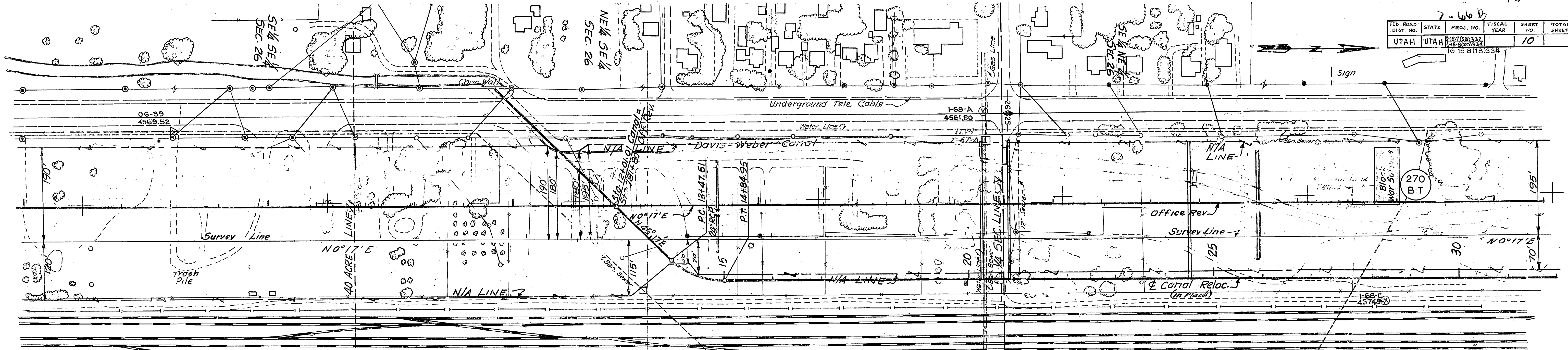






|                     |       |            |             |           |              |
|---------------------|-------|------------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO.  | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| UTAH                | UTAH  | 157(68)334 | 1968        | 10        |              |

T.V. Stevens Jr. 1961

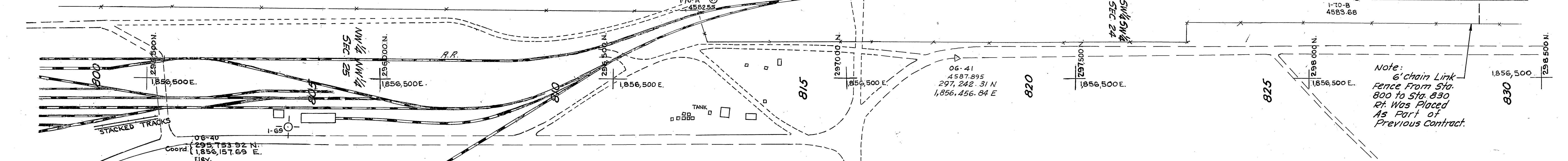
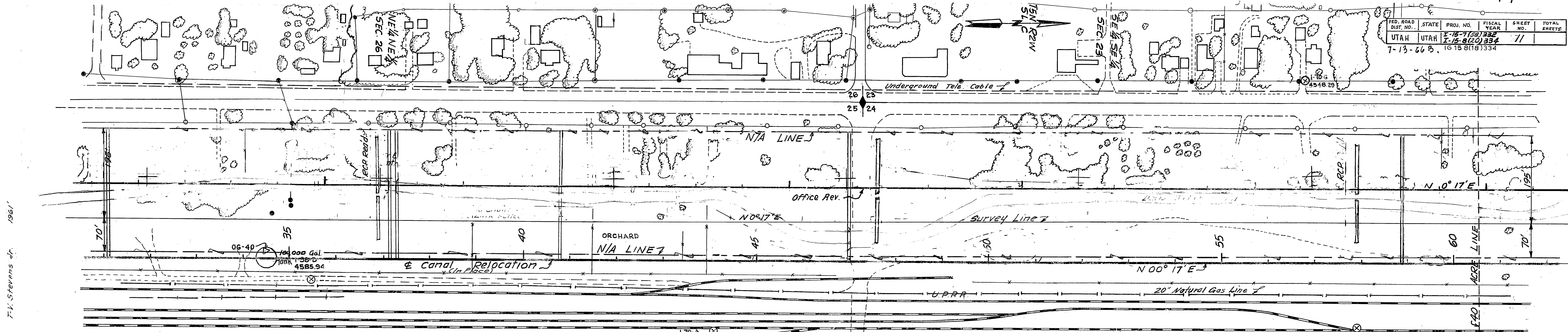


North Bound Lane

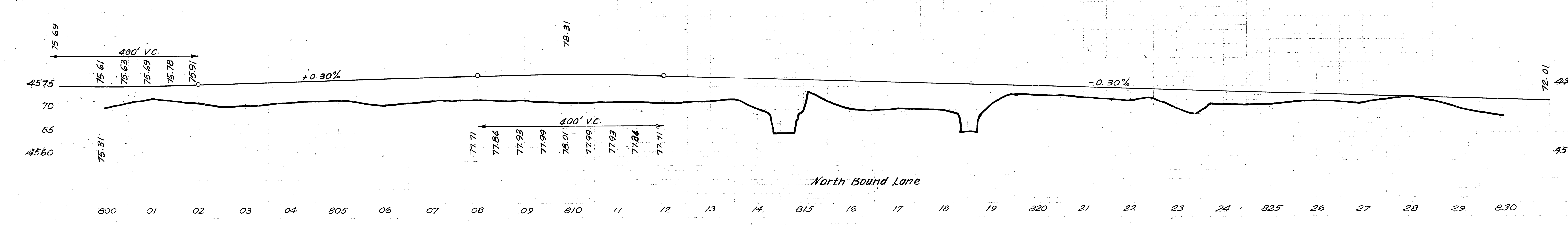
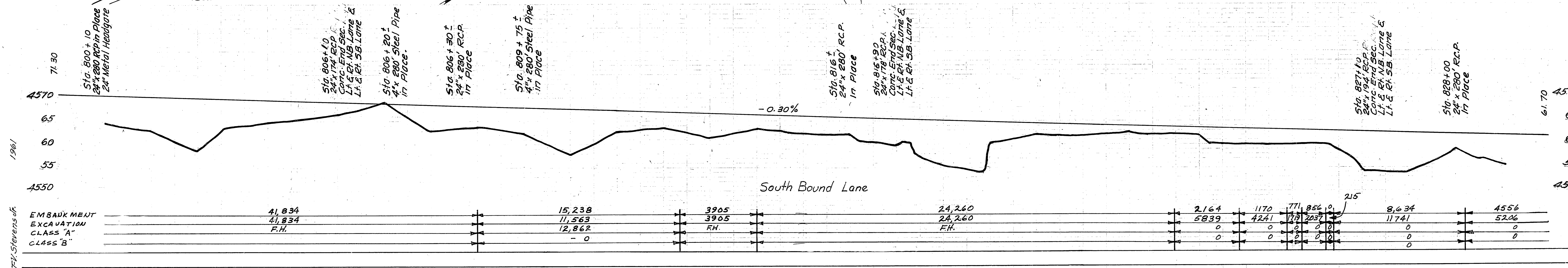


|                     |       |           |             |           |              |
|---------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| UTAH                | UTAH  | 7-15-153  | 1966        | 71        | 111          |

7-13-66 6, 16 15 8(18) 334

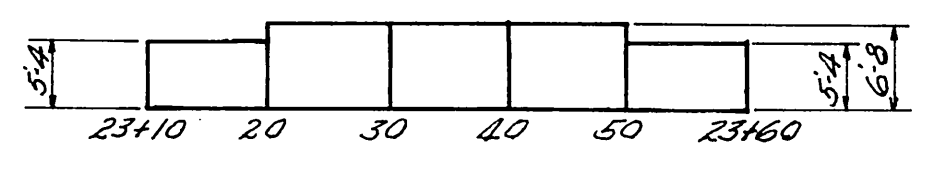
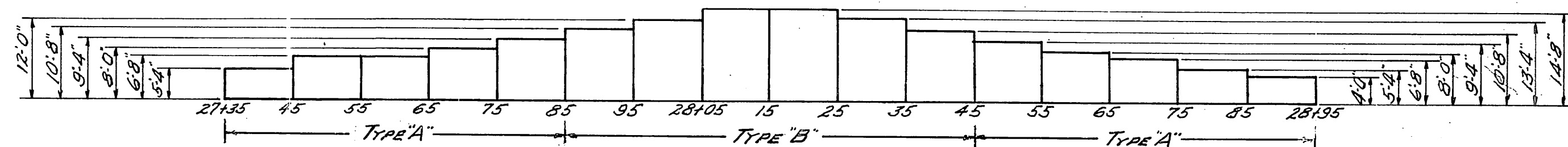
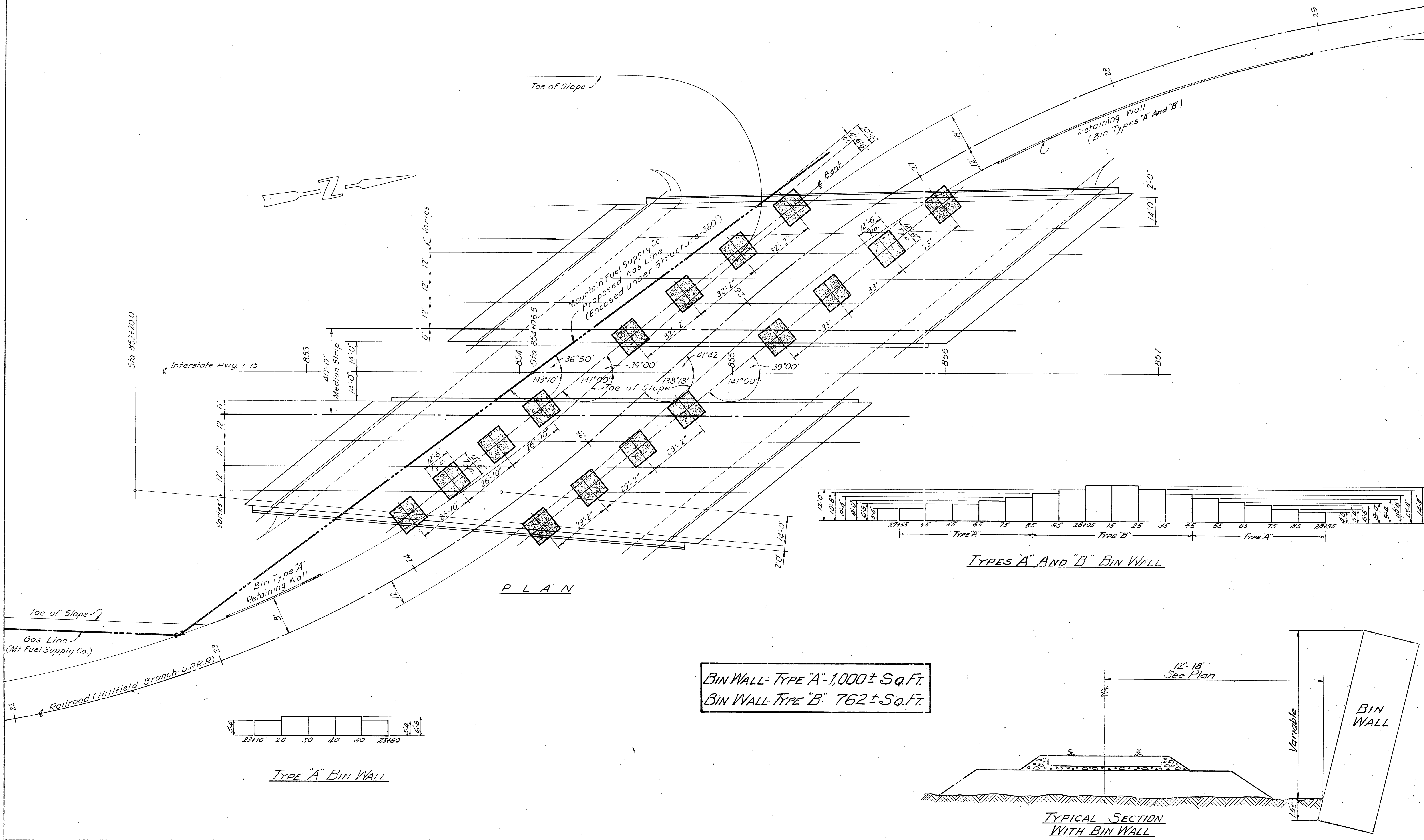


Note:  
6' chain Link Fence From Sta. 800 to Sta. 830 Rt. Was Placed As Part of Previous Contract.

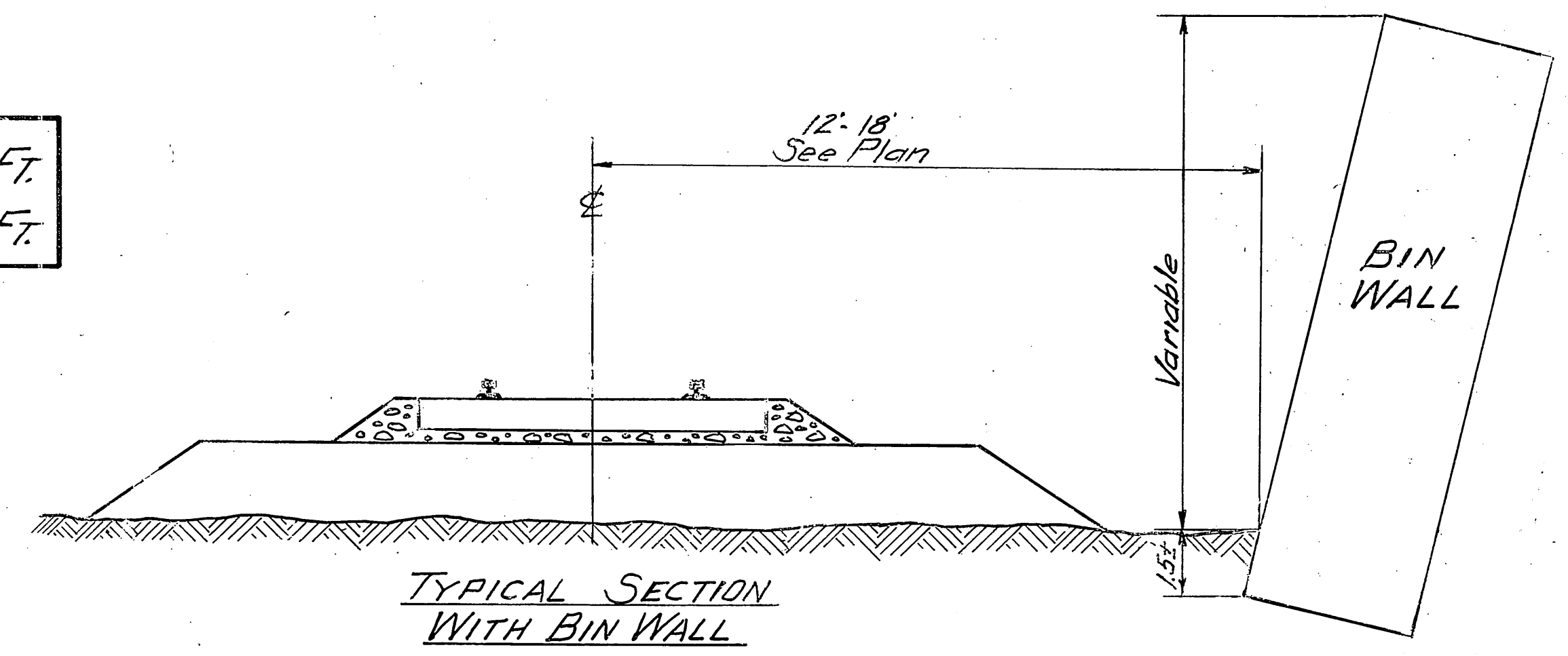


T.V. Stevens Jr. 1961

T.V. Stevens Jr. 1961



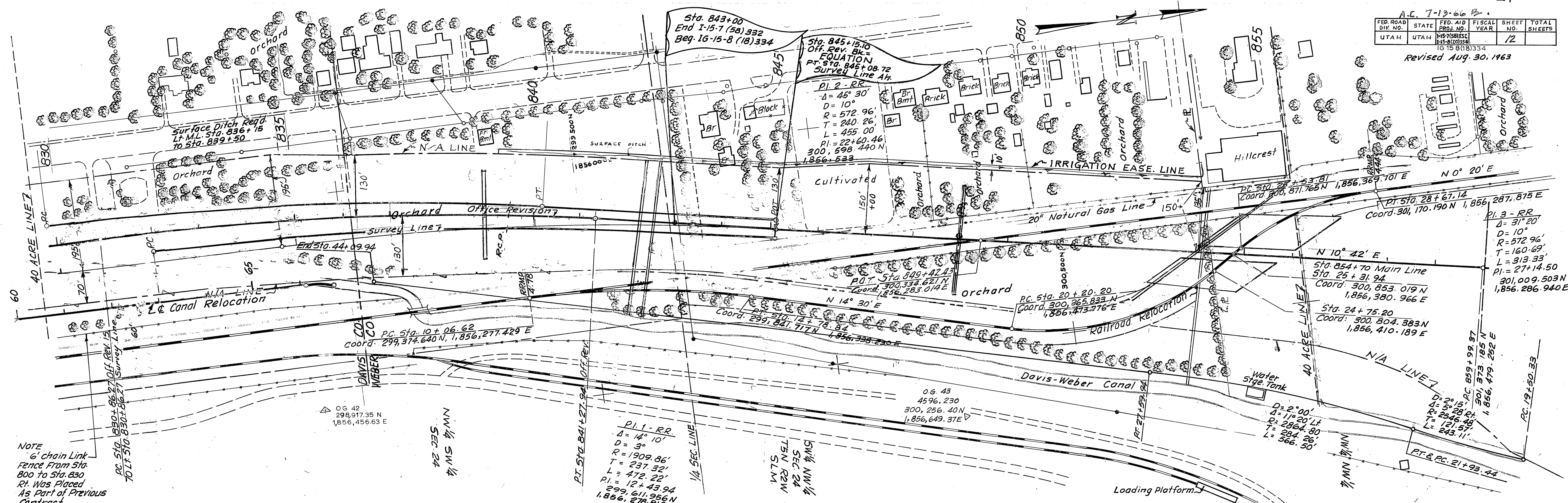
BIN WALL- TYPE "A"-1,000± Sq.Ft.  
BIN WALL- TYPE "B"- 762± Sq.Ft.





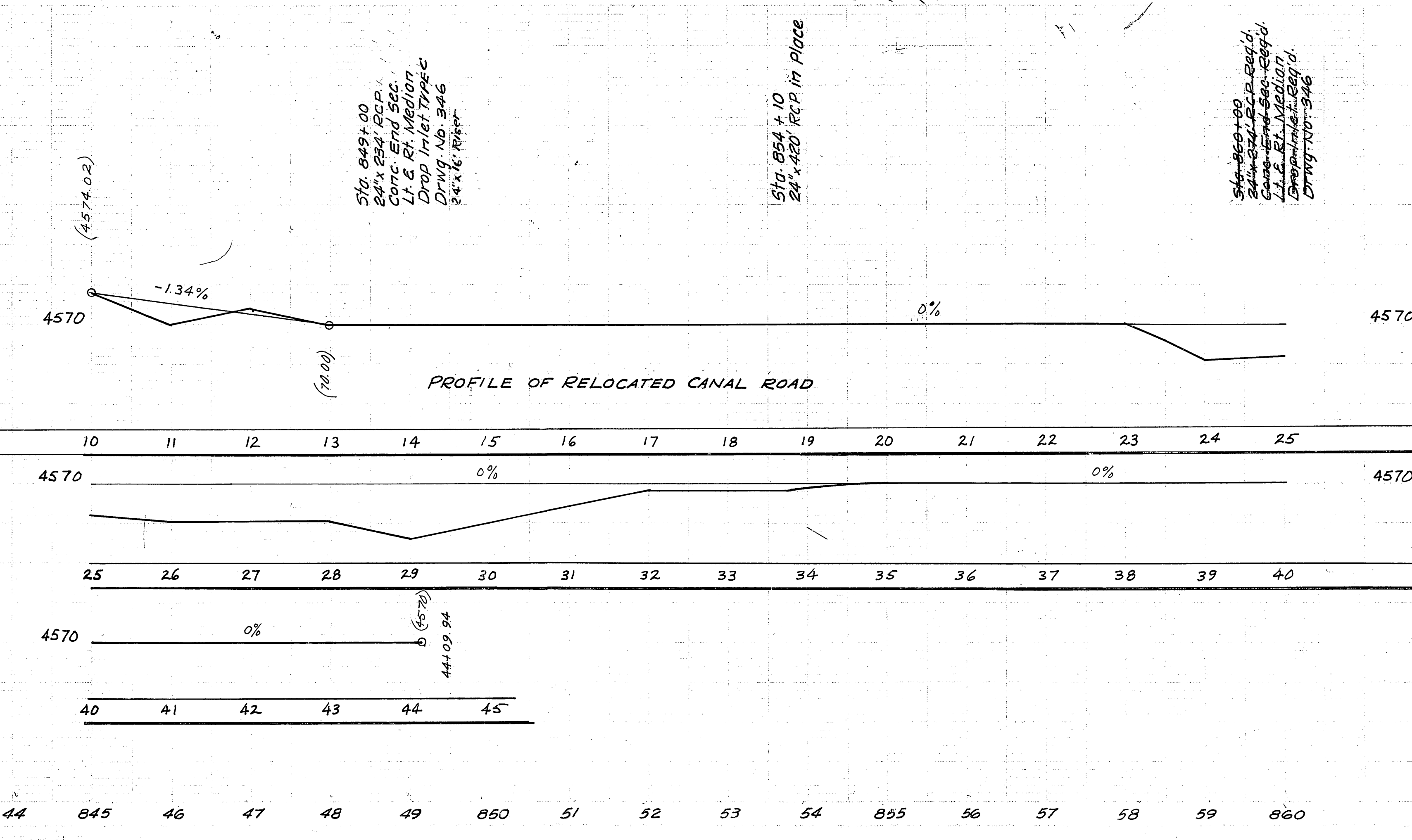
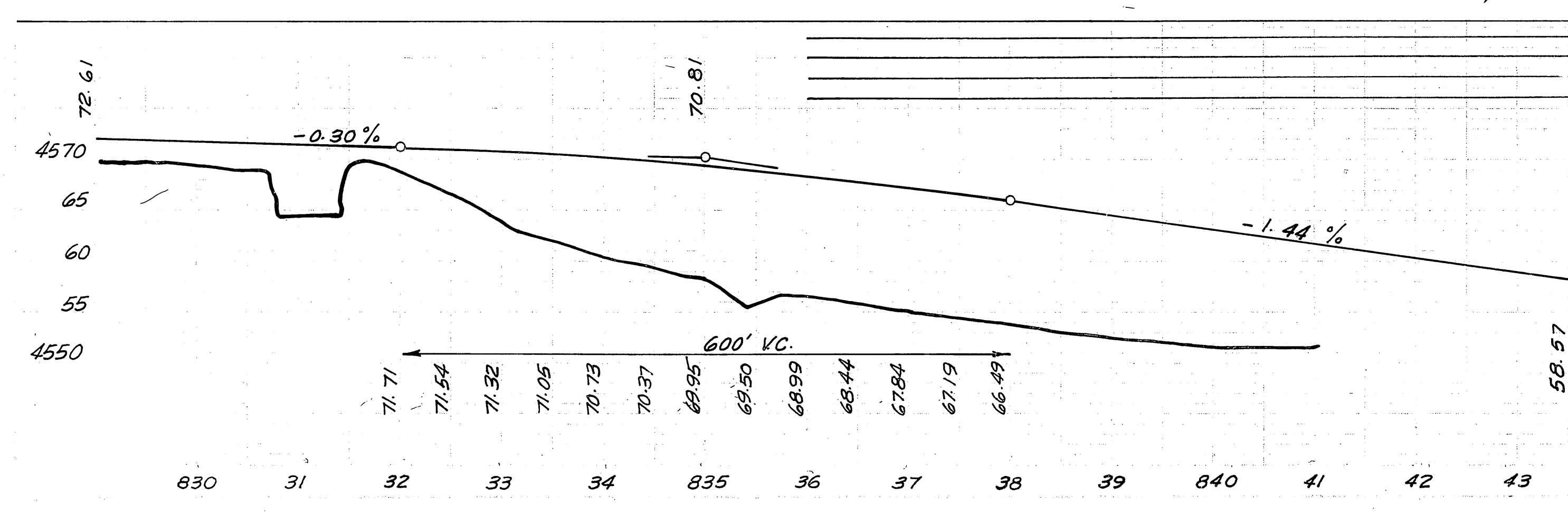
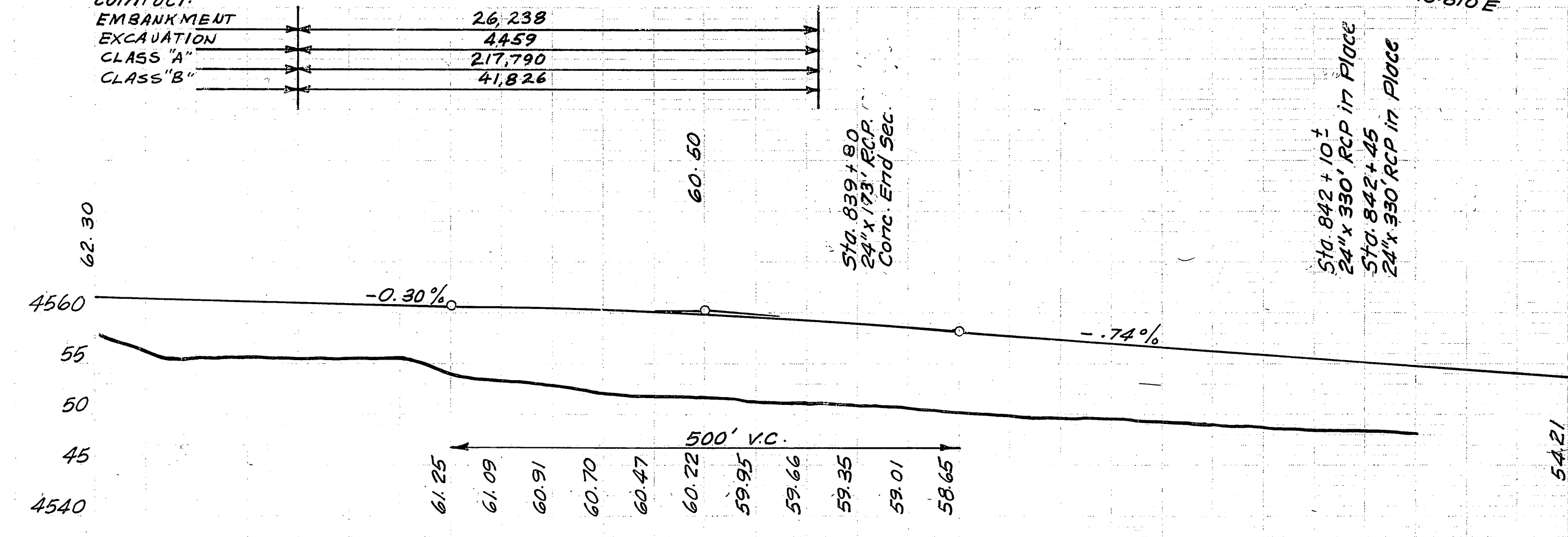
|                    |       |                    |             |           |              |
|--------------------|-------|--------------------|-------------|-----------|--------------|
| FED. ROAD DIV. NO. | STATE | FED. AID PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| UTAH               | UTAH  | 45-100333-1        | 1963        | 72        |              |

Revised Aug. 30, 1963



NOTE  
6' chain Link Fence From Sta. 800 to Sta. 830 Pt. Was Placed As Part of Previous Contract.

|            |         |
|------------|---------|
| EMBANKMENT | 26,238  |
| EXCAVATION | 4,459   |
| CLASS 'A'  | 217,790 |
| CLASS 'B'  | 41,826  |



Sta. 859+00  
24"x24" R.C.P.  
L.I.E. 12' Mediator  
Drop Inlet 17"x12"  
Drain. No. 346  
24"x16" Riser

Sta. 854+10  
24"x42" R.C.P. in Place

Sta. 849+00  
24"x24" R.C.P.  
Conc. End Sec.  
L.I.E. 12' Mediator  
Drop Inlet 17"x12"  
Drain. No. 346  
24"x16" Riser

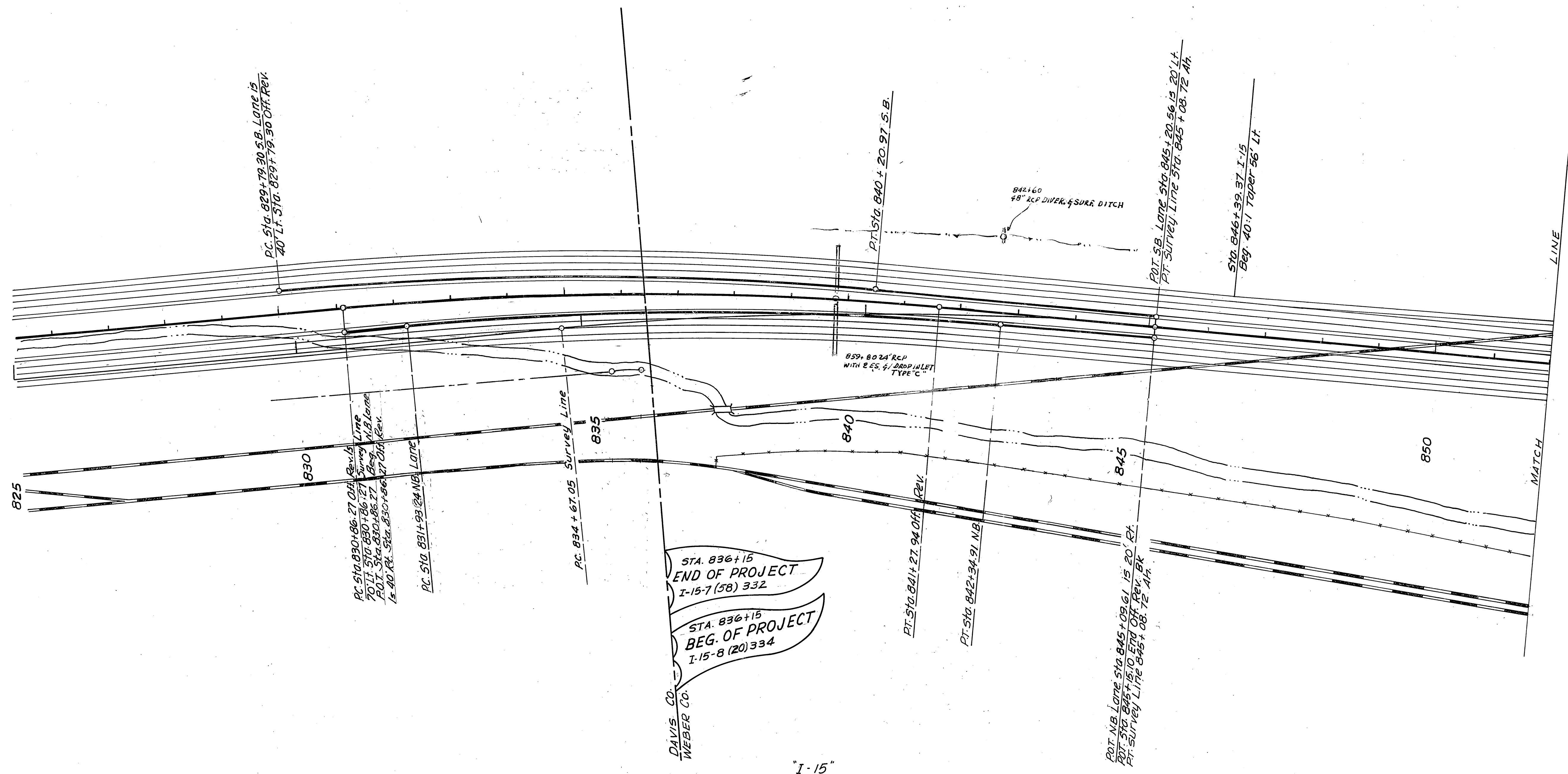
Sta. 842+10  
24"x30" R.C.P. in Place  
Sta. 842+45  
24"x30" R.C.P. in Place

Sta. 839+80  
24"x173" R.C.P.  
Conc. End Sec.

| 7-66B               |       |               |             |           |              |
|---------------------|-------|---------------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO.     | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| UTAH                | UTAH  | I-15-7(58)332 |             | 14        |              |
| I-15-8(20)334       |       |               |             |           |              |
| I-15-8(18)334       |       |               |             |           |              |



| REVISIONS |    |    |
|-----------|----|----|
| DATE      | BY | BY |
|           |    |    |
|           |    |    |
|           |    |    |



STA. 836+15  
END OF PROJECT  
I-15-7 (58) 332

STA. 836+15  
BEG. OF PROJECT  
I-15-8 (20) 334

DAVIS CO.  
WEBER CO.

"1-15"

$\Delta = 10^\circ 25' \text{ Rt.}$   
 $D = 1^\circ 00'$   
 $R = 5729.58'$   
 $T = 522.27'$   
 $L = 1041.67'$

PI. 9 Survey Line Sta. 839+89.32  
 Coord: 299, 395.29 N  
 1,856, 105.53 E

PI. 9 Off. Revision Sta. 836+08.54  
 Coord: 299, 014.87 N  
 1,856, 033.64 E

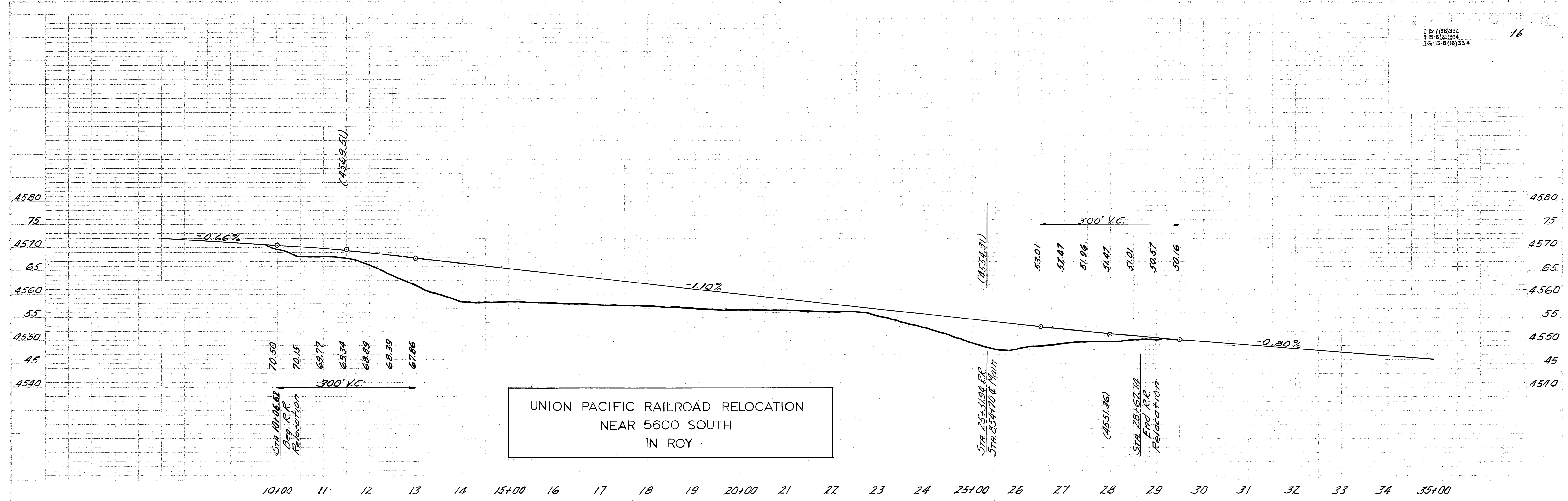
PI. 9 S.B. Sta. 835+01.57  
 Coord: 298, 908.09 N  
 1,855, 993.11 E

PI. 9 N.B. Sta. 837+15.51  
 Coord: 299, 121.64 N  
 1,856, 074.17 E









STA 10+00 to 11+00  
 R.R. Relocation  
 300' V.C.  
 70.50  
 70.15  
 69.77  
 69.34  
 68.89  
 68.39  
 67.86

STA 25+31.94 R.R.  
 STA 25+31.94 Main  
 300' V.C.  
 53.01  
 52.47  
 51.96  
 51.47  
 51.01  
 50.57  
 50.16

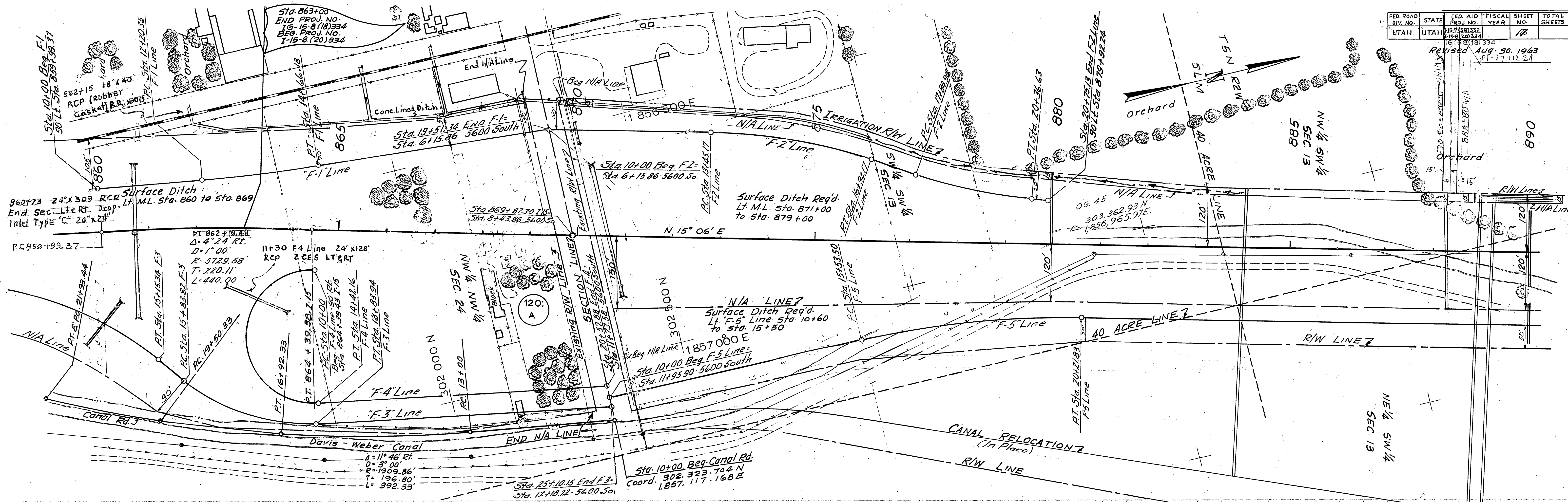
STA 28+67.14  
 End R.R.  
 Relocation  
 (4551.36)

UNION PACIFIC RAILROAD RELOCATION  
 NEAR 5600 SOUTH  
 IN ROY

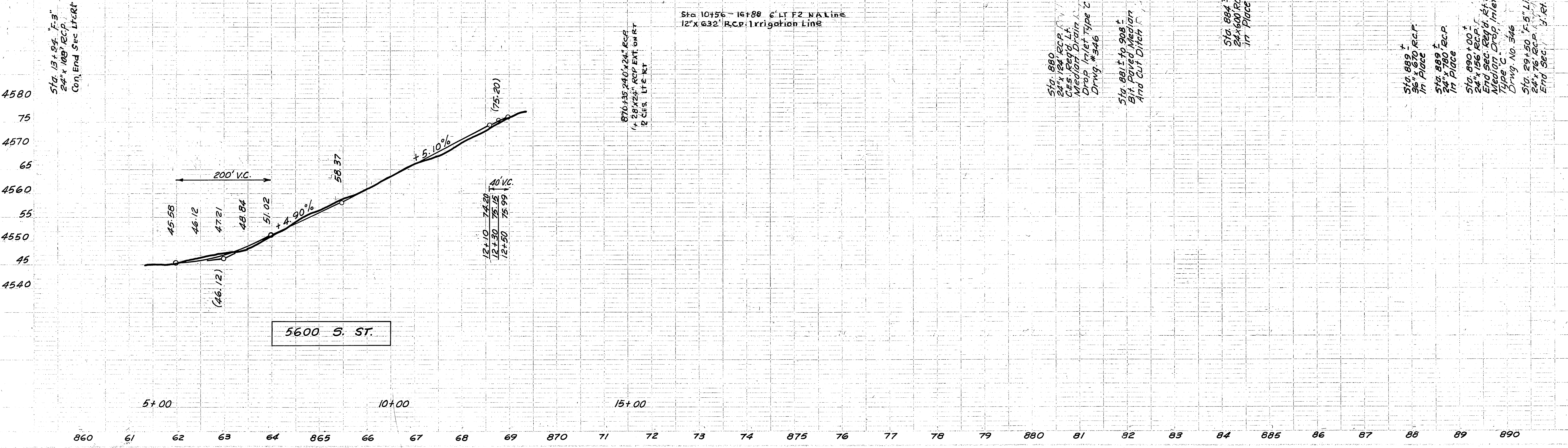


| FED. ROAD DIV. NO. | STATE | FED. AID PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|--------------------|-------|--------------------|-------------|-----------|--------------|
| UTAH               | UTAH  | 15-8(18)334        | 1963        | 13        | 18           |

Revised Aug. 30, 1963  
 15-8(18)334  
 15-8(20)334

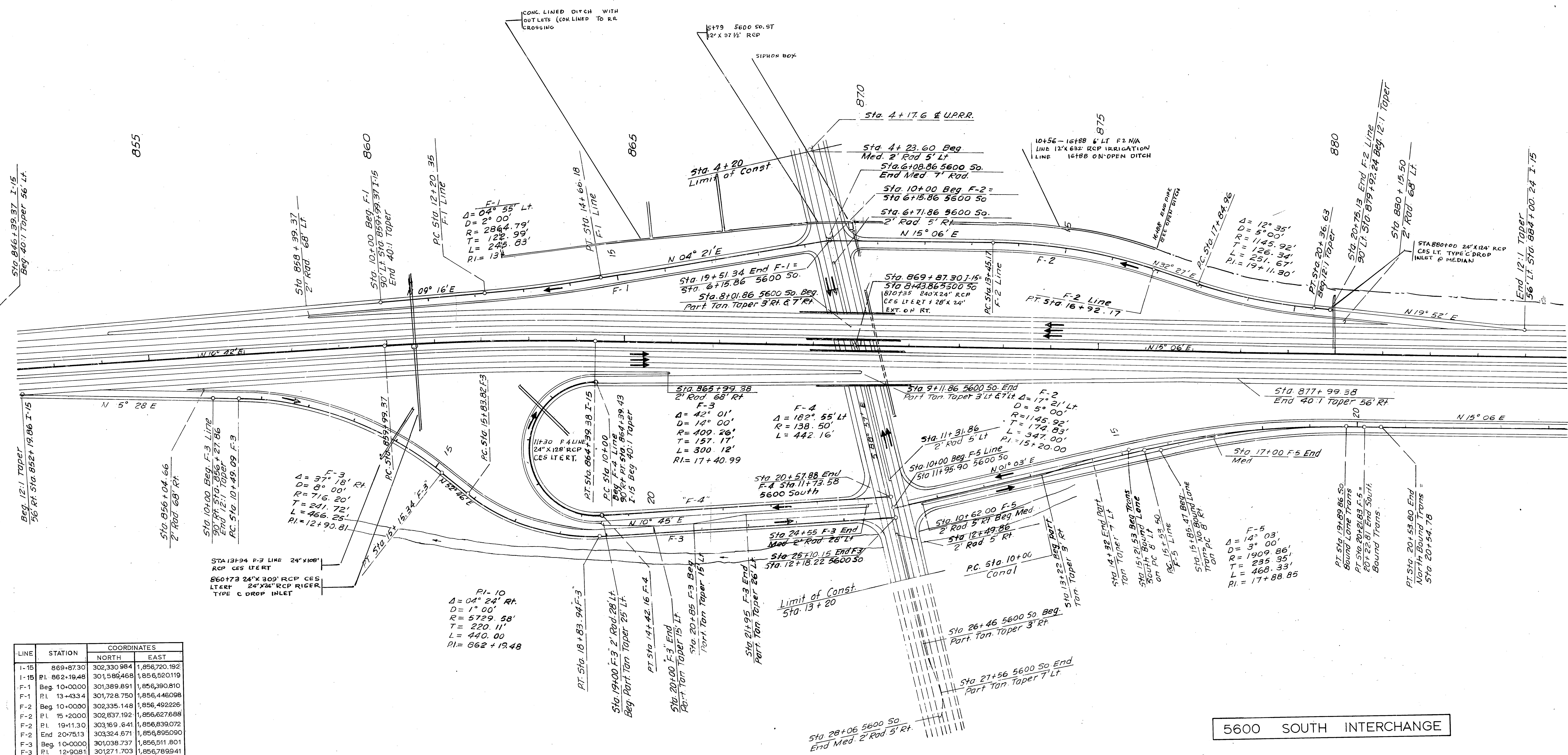
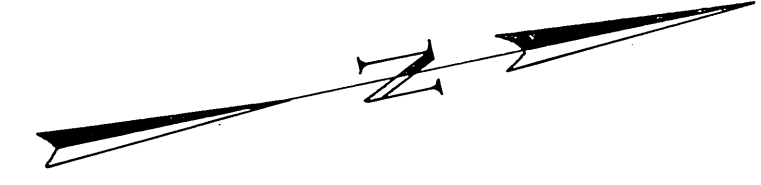


|            |
|------------|
| EMBANKMENT |
| EXCAVATION |
| CLASS A    |
| CLASS B    |





|                     |       |              |              |                 |              |
|---------------------|-------|--------------|--------------|-----------------|--------------|
| AC 7-14-66 13       |       | PROJ. NO.    | FISCAL YEAR  | SHEET NO.       | TOTAL SHEETS |
| FED. ROAD DIST. NO. | STATE | UTAH         | 1967         | 18              | 18           |
| UTAH                |       | 116-7(5) 131 | 115-6(2) 134 | 16 15 8(1) 1334 |              |



| LINE | STATION       | COORDINATES |               |
|------|---------------|-------------|---------------|
|      |               | NORTH       | EAST          |
| I-15 | 869+87.30     | 302,330.984 | 1,856,720.192 |
| I-15 | PI. 862+19.48 | 301,589.468 | 1,856,520.119 |
| F-1  | Beg. 10+00.00 | 301,389.891 | 1,856,390.810 |
| F-1  | PI. 13+43.34  | 301,728.750 | 1,856,446.098 |
| F-2  | Beg. 10+00.00 | 302,335.148 | 1,856,492.226 |
| F-2  | PI. 15+20.00  | 302,837.192 | 1,856,627.688 |
| F-2  | PI. 19+11.30  | 303,169.641 | 1,856,839.072 |
| F-2  | End 20+75.13  | 303,324.671 | 1,856,895.090 |
| F-3  | Beg. 10+00.00 | 301,038.737 | 1,856,511.801 |
| F-3  | PI. 12+90.81  | 301,271.703 | 1,856,789.941 |
| F-3  | PI. 17+40.99  | 301,554.492 | 1,856,948.371 |
| F-3  | End 25+10.15  | 302,324.124 | 1,857,094.490 |
| F-4  | Beg. 10+00.00 | 301,778.586 | 1,856,664.362 |
| F-4  | Cur. Curve    | 301,745.862 | 1,856,789.941 |
| F-4  | End 20+57.88  | 302,324.942 | 1,857,049.857 |
| F-5  | Beg. 10+00.00 | 302,324.529 | 1,857,072.176 |
| F-5  | PI. 17+88.85  | 303,113.246 | 1,857,086.632 |

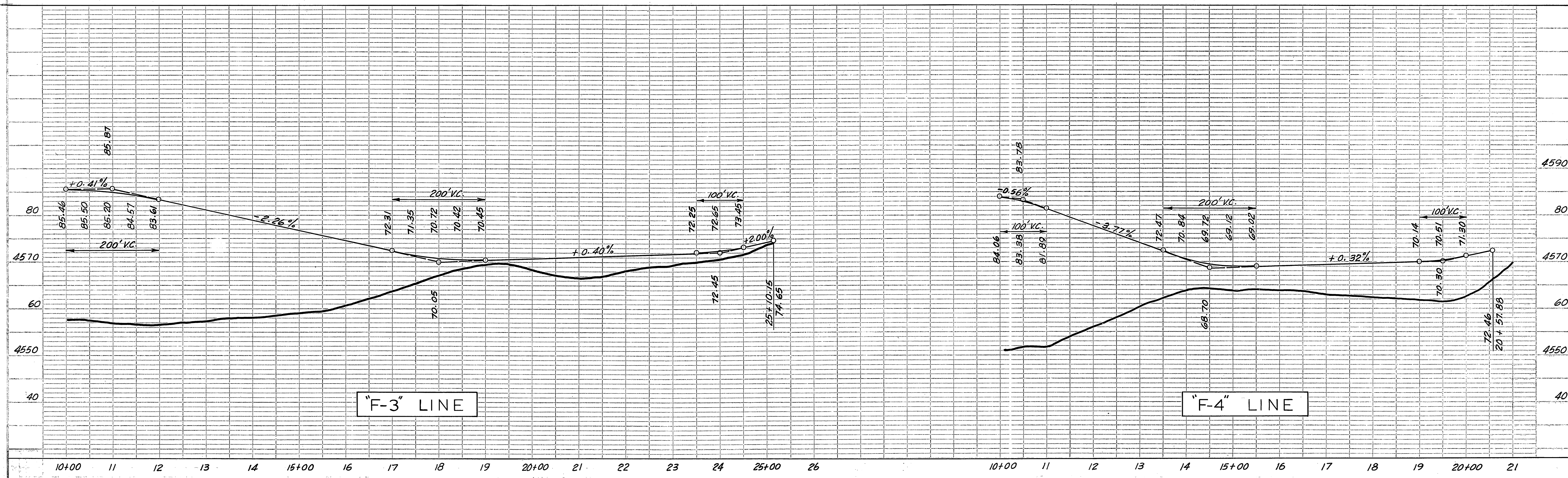
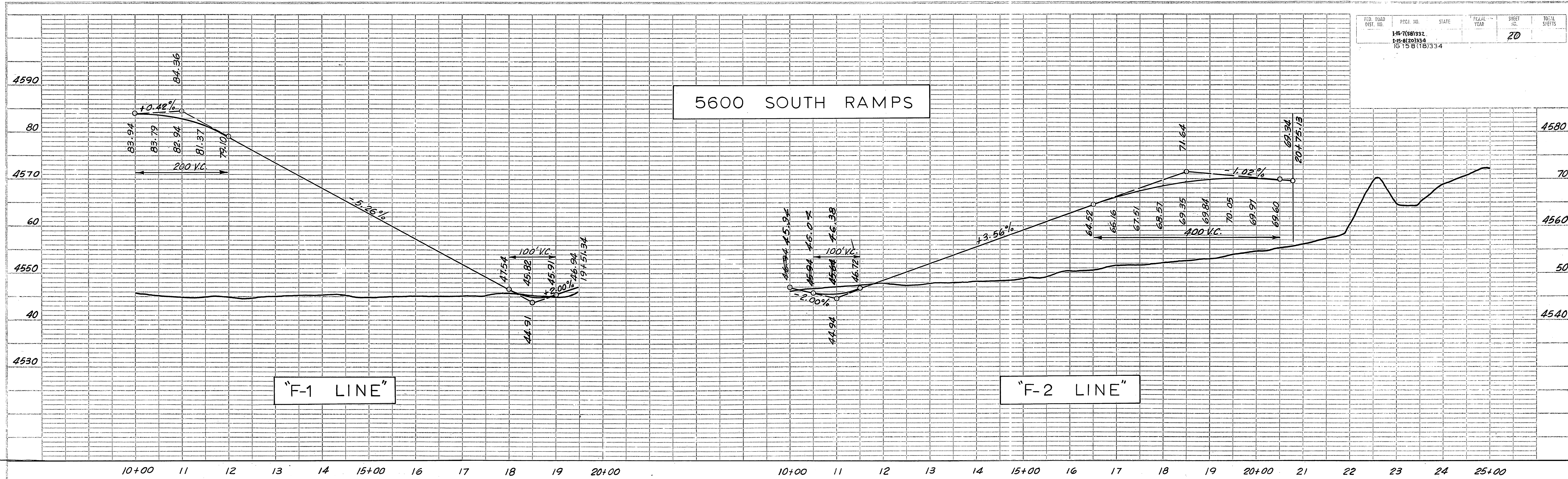
5600 SOUTH INTERCHANGE





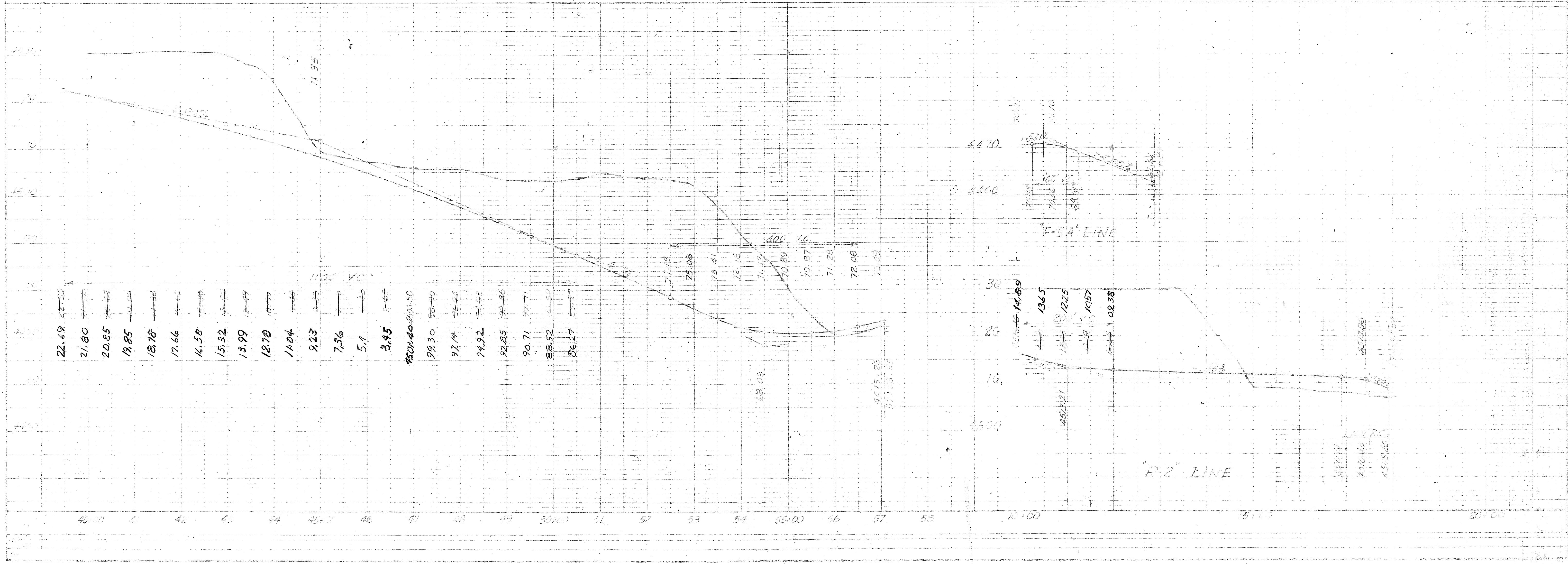
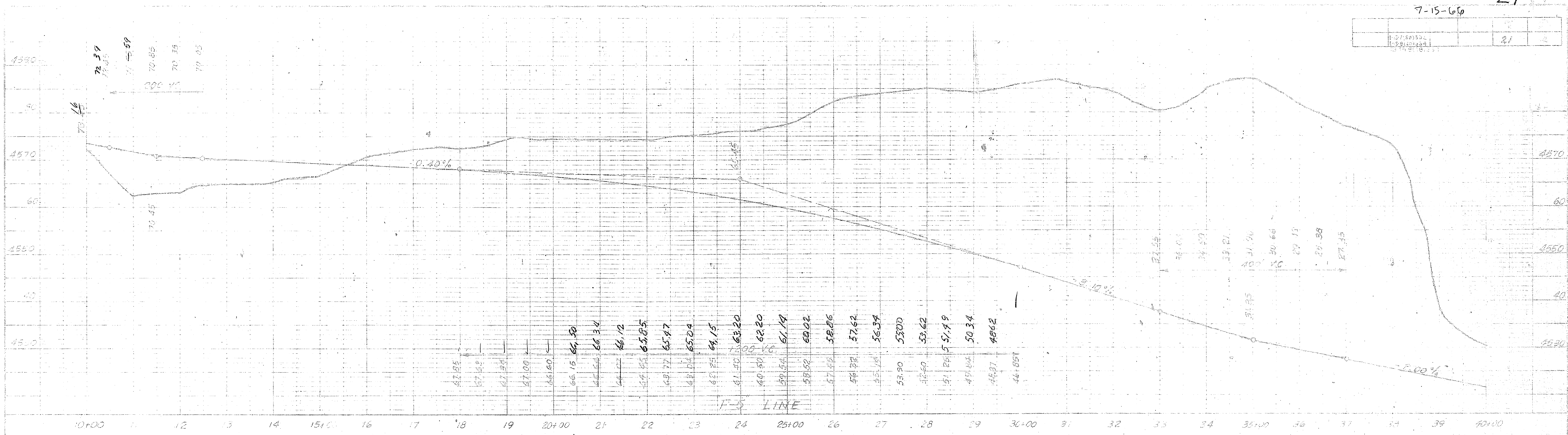


### 5600 SOUTH RAMPS





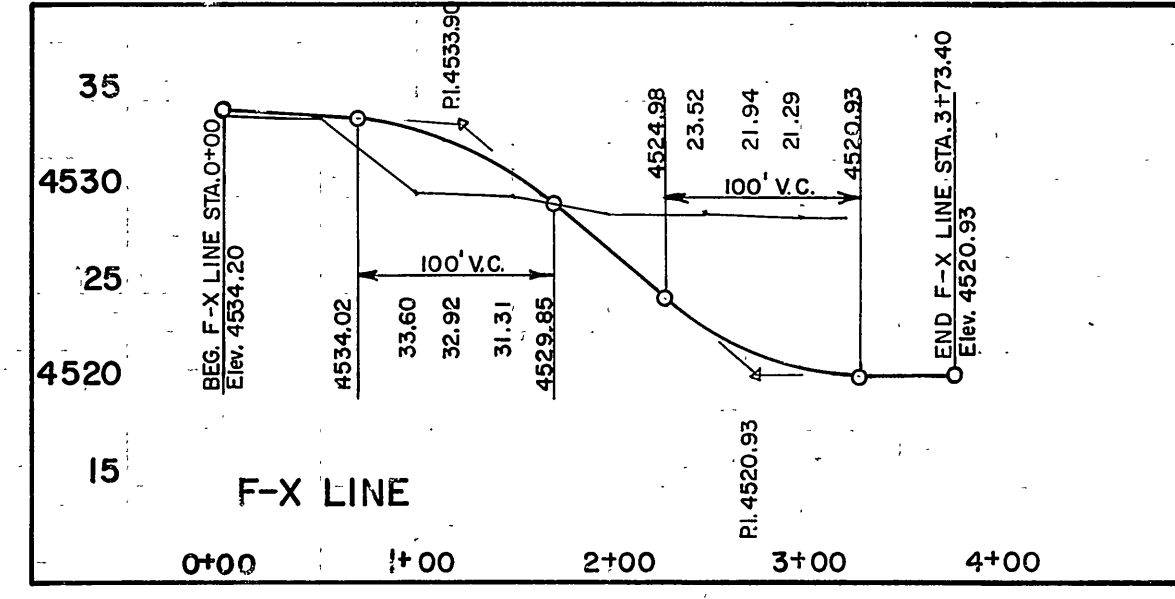
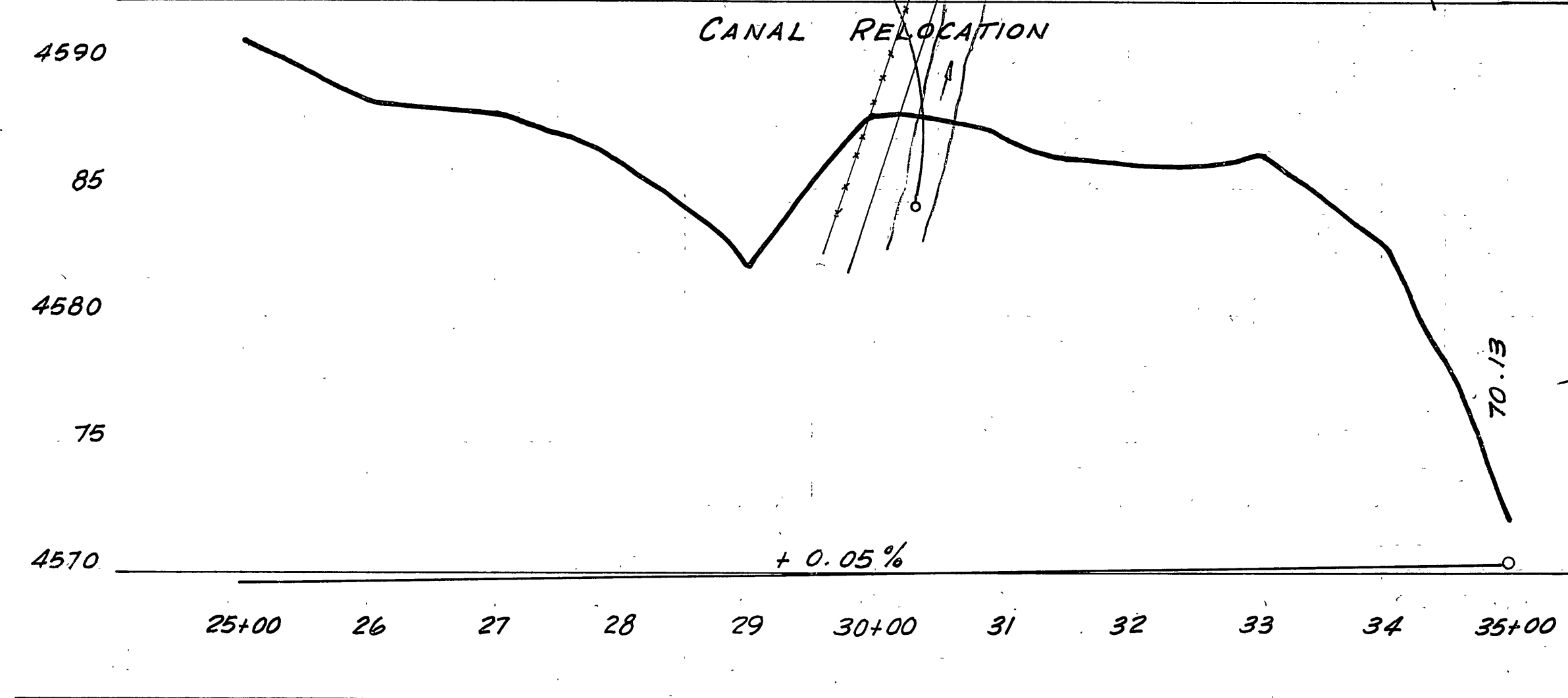
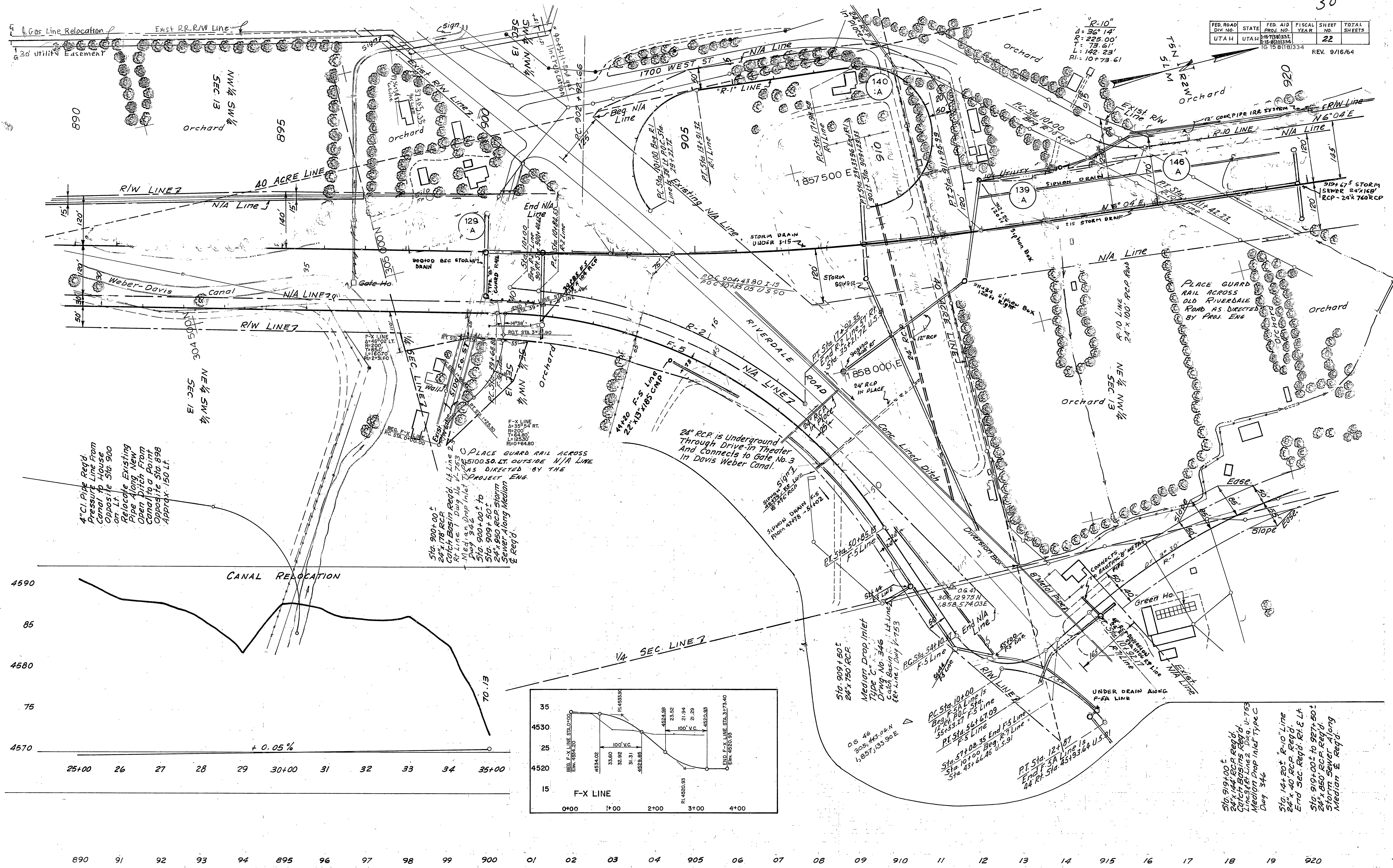
|              |    |
|--------------|----|
| 1-27-34-53-2 | 21 |
|--------------|----|





|                    |       |                    |             |           |              |
|--------------------|-------|--------------------|-------------|-----------|--------------|
| FED. ROAD DIV. NO. | STATE | FED. AID PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| UTAH               | UTAH  | 44-708(33)         | 1964        | 22        | 16           |

REV. 9/16/64



890 91 92 93 94 895 96 97 98 99 900 01 02 03 04 905 06 07 08 09 910 11 12 13 14 915 16 17 18 19 920

Sta. 919+00 ±  
24" x 14" R.C.P. Req'd.  
Catch Basins Req'd.  
Line 2, Dig. U-753  
Median Drop Inlet Type C  
Dwg. 346

Sta. 14+20 ± R-10 Line  
24" x 40 R.C.P. Req'd.  
End Sec. Req'd. Rt. & Lt.

Sta. 919+00 ± to 927+50 ±  
24" x 850 R.C.P. Req'd.  
Storm Sewer Along  
Median & Req'd.

PLACE GUARD RAIL ACROSS  
5100.50 LT. OUTSIDE N/A LINE  
AS DIRECTED BY THE  
PROJECT ENG.

Sta. 900+00 ±  
24" x 178 R.C.P.  
Catch Basin Req'd.  
Rt. Line 1 Dig. U-753  
Median Drop Inlet  
Dwg. 346  
Sta. 900+00 ± to  
Sta. 909+50 ±  
24" x 950 R.C.P. Storm  
Sewer Along Median  
& Req'd.

24" R.C.P. is Underground  
Through Drive-in Theater  
And Connects to Gate No. 3  
in Davis Weber Canal.

4" CI Pipe Req'd  
Pressurized Line From  
Carrar to House  
Opposite Sta. 900  
on Lt.  
Relocate Existing  
Pipe Along New  
Open Ditch From  
Canal to a Point  
Opposite Sta. 898  
Approx. 150 Lt.



